

Chapter 03 Global Regulations and Design Provisions

03.01.000 Purpose and Intent

The purpose of this Chapter is to provide regulations and design guidelines that are applicable to multiple districts to supplement those included in Chapter 02, District Regulations and Design Provisions. This Chapter includes regulations and design guidelines addressing the following matters:

- a. Permitted, prohibited, and temporary uses;
- b. Street standards;
- c. Pedestrian corridor standards and guidelines;
- d. Overall design character and urban form standards;
- e. Architectural standards and guidelines;
- f. Landscape architectural standards and guidelines;
- g. Signing regulations and guidelines;
- h. Parking and access management standards and guidelines; and,
- i. Performance standards.

The provisions included herein provide overall standards and guidelines for the *EUC*. In the event that the standards and guidelines found herein are in conflict with a more specific provision in Chapter 02, the standard or guideline in Chapter 02 shall prevail.

03.02.000 Permitted Uses

All districts in the *EUC* permit multiple uses and most are required to contain more than a single use. This flexibility is limited by the requirement for areas to have a dominant land use character. Exhibit III-1 (Regulating Plan - Dominant Land Use), indicates the dominant land use character required. This character is the basis for permitted and not permitted uses. A description of each dominant land use type follows and is illustrated in Exhibits III-2 through III-11 (Building Typology). Chapter 02 contains additional requirement specific to each district. Any use consistent with the applicable building type district description for its proposed location, as provided below, shall be permitted. Uses not permitted are listed in Section 03.04.

03.02.001 Mixed Use District:

The mixed use district shall be characterized by mixed uses in vertical or horizontal arrangements. The street level on prominent streets shall be predominately retail commercial or office uses, except the Eastern Gateway mixed use district need not have non-residential. Eating and drinking establishments, including those with entertainment, are permitted. Such establishments need to conform to the performance criteria herein and the CVMC. Streetscape uses including outdoor sales, cafes, and concessions such as street vendors in the nearby town square and civic plaza are appropriate in this district.

03.02.002 Business/Retail Dominant District:

This district allows mixed uses as described in the mixed use district description but also allows single use structures for offices, retail, hotels, and business support services. Residential and civic uses are also permitted in mixed use and stand alone formats, but within the district shall be secondary to business and retail activities. Streetscape uses including concessions such as street vendors in the nearby town square are appropriate in this district.

In addition to the dominant land uses of the district, the inclusion of services such as child care facilities and adult education, which complement or support the primary land uses contemplated for the district, should be encouraged either as stand-alone formats or as a part of large employment users. Childcare facilities provided in either of these formats will qualify for credit as Community Purpose Facilities.

03.02.003 Mixed Use/Civic Dominant District:

This is similar to the business/retail dominant district, but the emphasis shall be on civic activities and buildings such as the library, government offices, and quasi-public uses. These uses need not consume the majority of floor area in the district but the majority of all such uses located in the *EUC* shall be located within this district. Streetscape uses including concessions such as street vendors in the civic plaza park are appropriate in this district.

In addition to the dominant land uses of the district, the inclusion of services such as child care facilities which complement or support the primary land uses contemplated for the district, should be encouraged either as stand-alone formats or as a part of large employment users. Childcare facilities provided in either of these formats will qualify for credit as Community Purpose Facilities.

03.02.004 Residential Dominant Districts:

There are six distinct residential districts within the *EUC*, four surrounding a centralized park facility. Each shall be a predominately residential district. However convenience retail and personal service uses shall be permitted on the first floor of buildings with residential uses above. Additionally, residential live-work, flex-space or loft units, are permitted. These combined uses will be categorized according to the amount of square footage of non-residential, and number of residential units included for intensity calculations. Flex-space will be considered non-residential.

03.03.000 Building Type District Boundaries

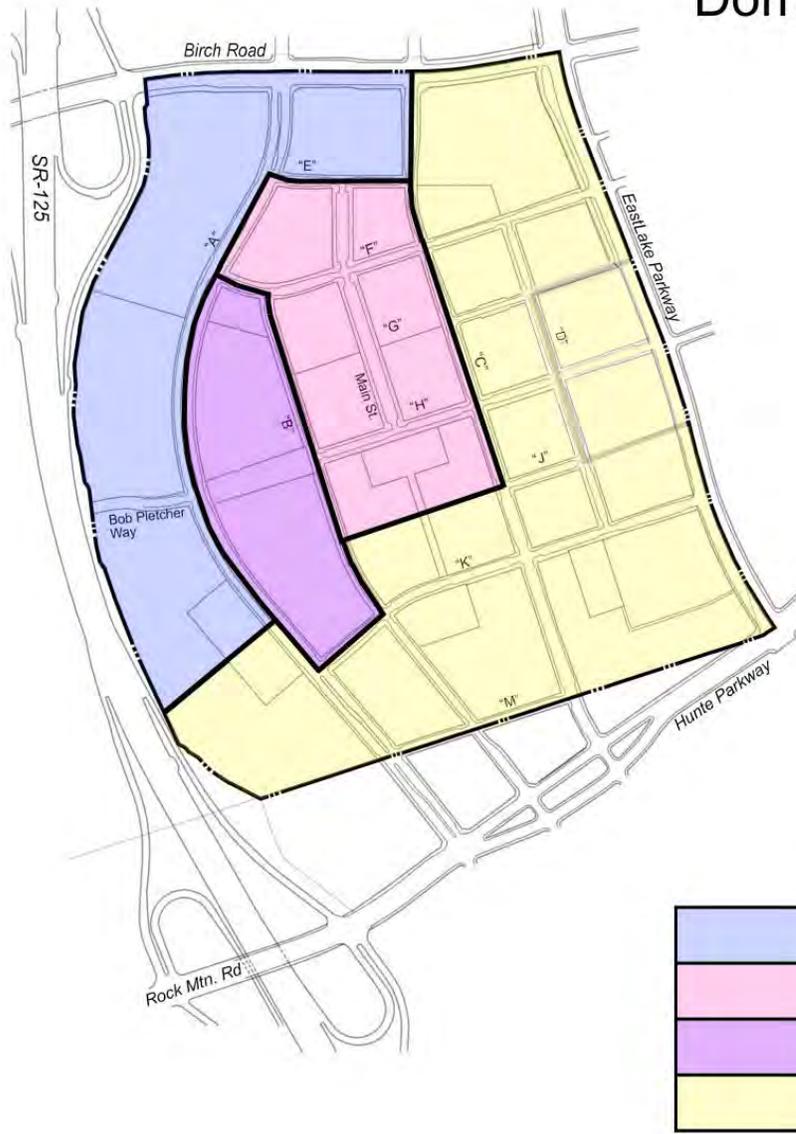
The boundaries of Building type districts within the *EUC SPA* are shown on Exhibit III-1 (Regulating Plan - Dominant Land Use). The Building type district boundaries shown on the map coincide with the right-of-way edges of proposed streets, alleys or lot lines. Changes to these boundaries resulting from the relocation of a boundary street, alley, right-of-way, or lot line by the approval of a tentative or final subdivision map shall be incorporated in the Building Type Districts

Map as an administrative matter by the Director of Planning & Building, subject to the following findings:

- 03.03.001 That the boundary adjustment enhances, or does not distract from, the character intent established in Chapter II, herein;
- 03.03.002 That the boundary adjustment does not unduly constrain pedestrian circulation, and;
- 03.03.003 That the boundary adjustment will not result in any substantive modification of the otherwise permitted intensity ranges.

Regulating Plan

Dominant Land Use



Eastern Urban Center OTAY RANCH



Exhibit III-1

Building Typology

Mixed Use



Effective use of Iconic Building and Encroachments
in an Office over Retail Environment



Encroachments into a
Pedestrian Corridor



Pedestrian Friendly Mid-block Crossing



Eastern Urban Center
OTAY RANCH



Exhibit III-2

Building Typology

Mixed Use



Effective Corner Treatment in a Mixed Use Building



Using Dramatic Building Elements to define access point



An Activated Town Square Park in a Mixed Use Setting



Eastern Urban Center
OTAY RANCH



Exhibit III-3

Building Typology

Mixed Use



Combining multi-use buildings and styles



Defining Pedestrian Crossings



Mixed Use with Pedestrian Scaled Features



Eastern Urban Center
OTAY RANCH



Exhibit III-4

Building Typology

Mixed Use



A pedestrian friendly environment created with landscaping and outdoor dining



Town Square is activated with outdoor dining and building entries



Eastern Urban Center OTAY RANCH



Exhibit III-5

Building Typology

Business/Retail



Creating a Campus Atmosphere



Offices above Service Retail



Using Building Forms to define Entry



Retail & Office with Mixed Heights



Offices with an Internal Plaza



Varying Heights in Office Retail District



Eastern Urban Center
OTAY RANCH



Exhibit III-6

Building Typology

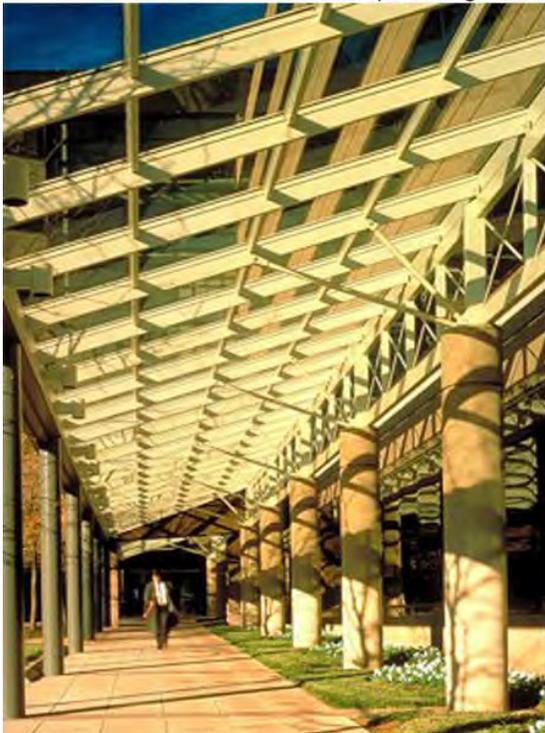
Business/Retail



Defining an entry, may require encroachments into public right-of-way



Creating off-street pedestrian orientation



Defining and connecting a pedestrian entry



Defining uses with design elements and materials



Eastern Urban Center
OTAY RANCH



Exhibit III-7

Building Typology

Mixed Use/Civic



Stepping back building Forms to add Interest & Light



Using slopes to complement a building



Connecting an Indoor Public Space to an Outdoor Public Space



Eastern Urban Center
OTAY RANCH



Exhibit III-8

Building Typology

Residential



Setbacks may vary to create plazas



Varying Heights and Building Forms
in Residential Building Types



Residential with Internal Paseo



Urban character maintained even with
added setback for and front yard patio.



Building line adjusted for trail



Eastern Urban Center
OTAY RANCH



Exhibit III-9

Building Typology

Residential



Residential Balconies



Residential Balconies



Varying Styles and Setbacks



Residential Stoops



Eastern Urban Center
OTAY RANCH



Exhibit III-10

Building Typology

Residential



Entry definition



Using Colors and Materials to create Interest



Color, height, and Form used to Create Variety



Good Pedestrian Orientation



Eastern Urban Center
OTAY RANCH



Exhibit III-11

03.04.000 Uses Not Permitted

In general, any use which is inconsistent with the *EUC* design and character goals of a high quality, high intensity/density, pedestrian oriented environment may be prohibited. The following uses are specifically prohibited within the Otay Ranch Eastern Urban Center because they are incompatible with the desired charter and intensity of the community:

- 03.04.001 Residential: Conventionally formatted single family detached dwelling units;
- 03.04.002 Commercial: Large format (>50,000 square feet on one *floor*) retail; automobile repair or full automotive service station; towing or storage yards; or industrial warehousing;
- 03.04.003 Industrial: All heavy manufacturing and assembly uses; warehousing; yards and terminals, and all permanent outdoor storage uses; and,
- 03.04.004 Agricultural: All permanent agricultural uses, except community gardens, as defined in Section 19.04.010 CVMC.

03.05.000 Temporary Uses and Special Events

- 03.05.001 Temporary Uses Permitted: The following temporary uses are subject to administrative approval by the Zoning Administrator, except as noted.
- a. Individual circuses, rodeos, parades, community or neighborhood celebrations, special events or similar outdoor entertainment or enterprises requiring closure of a public street, subject to not more than five days of operation in any calendar month. Requests exceeding these time limitations will require the submittal and approval of a Conditional Use Permit.
 - b. Christmas tree sales, Halloween pumpkin sales and other holiday sales or specific events subject to not more than forty days per event of site occupation.
 - c. Subdivision sales offices, sales information centers, sales pavilions, and model home complexes and signage located within the project, subject to the following minimum requirements. If the sales office is within an existing building, either on-site or an approved off-site location, no temporary use permit is required.
 - i. Trailers may be used for no more than 180 calendar days or until such time as the sales offices have been completed, whichever is less.
 - ii. An asphaltic or concrete paved parking lot, or on-street parking, shall provide sufficient parking spaces to accommodate said use.
 - iii. Faithful performance bonding, in an amount appropriate to guarantee removal and/or conversion of the sales office and attendant facilities shall be required.
 - iv. Other conditions that the Zoning Administrator deems necessary to ensure that the sales office will not constitute or be objectionable to the other uses in the neighborhood.

- d. Outdoor art and craft shows and exhibits, subject to not more than three calendar days of operation or exhibition in any sixty calendar day period in residential districts (2,3,8,9, and 10), but are permitted without restriction in all other districts.
 - e. Contractors' offices and storage yards on the site of an active construction project.
 - f. Mobile home residences for security purposes on the site of an active construction project.
 - g. Seasonal retail sales of agricultural products (farmers market, fruit and vegetable stands) require a TUP only if for periods greater than ninety days per calendar year in any single location.
 - h. Temporary use of properly-designated mobile trailer units for classrooms, offices, banks, *etc.*, for periods not to exceed ninety days subject to Zoning Administrator approval. Requests for such uses of more than ninety days in duration shall require the approval of a Conditional Use permit by the Planning Commission. Such units shall meet all necessary requirements of building, fire and health codes.
 - i. Other commercial uses as developed and operated in accordance with the guidelines in the Eastern Urban Center Parks, Recreation Open Space and Trails Master Plan, such as; private retail kiosks, street vendors, food stands, or other similar uses within publically owned areas. Such areas that are privately owned are not required to obtain a temporary use permit.
 - k. Off-site construction materials storage and processing and assembly during construction period.
 - l. Temporary surface parking lots that will be in use for less than three years.
 - m. Additional uses determined to be similar to the foregoing in the manner prescribed by these regulations.
- 03.05.002 Interim Uses: Interim uses are temporary uses that are approved for a period not greater than three years. Interim uses require Design Review approval as provided for in Chapter IV, or may be approved as part of a Block Build-out Plan.
- 03.05.003 Permits and Bonds: All temporary uses, as described in Chapter 03.05.01, shall be subject to the issuance of a Temporary Use Permit by the Zoning Administrator and other necessary permits and licenses, including but not limited to, building permits, sign permits and solicitors or vending licenses. In the issuance of such a permit, the Zoning Administrator shall indicate the permitted hours of operation and any other conditions, such as walls, fences or lighting, which are deemed necessary to reduce possible detrimental effects to surrounding developments and to protect the public health, safety and welfare. Prior to the issuance of a permit for a temporary use, a cash deposit may be required to be deposited with the City. This cash deposit shall be used to defray the costs of property cleanup by the City in the event the permit fails to do same.

- a. Extension or Modification of Limits: Upon written application, the Zoning Administrator may extend the time within which temporary uses may be operated, or may modify the limitations under which such uses may be conducted if the Zoning Administrator determines that such extension or modification is in accord with the purposes of the zoning regulations.
- b. Condition of Site Following Temporary Use: Each site occupied by a temporary use shall be left free of debris, litter or any other evidence of the temporary use upon completion or removal of the use, and shall thereafter be used only in accord with the provisions of the zoning regulations.

03.05.004 Fee: The application shall be accompanied by a fee established by the Master Fee Schedule to cover the cost of processing the application prescribed in this section.

03.06.000 Home Occupations

Home occupations may be permitted only when in compliance with the conditions listed herein. A permit must be issued by the Zoning Administrator prior to operation of such use. The fee shall be in accordance with the Master Fee Schedule.

03.06.001 There shall be no stock in trade or exterior storage of materials in the conduct of home occupation.

03.06.002 A home occupation shall be conducted entirely within a dwelling; if in garage, it shall not impede the use of said garage for vehicle storage.

03.06.003 Electrical or mechanical equipment which creates visible or audible interference in radio or television receivers, or causes fluctuations in line voltage outside the dwelling unit, shall be prohibited.

03.06.004 No one other than the residents of the dwelling unit may be engaged in the conduct of the home occupation.

03.06.005 The establishment and conduct of a home occupation shall not change the principal character or use of the dwelling unit involved.

03.06.006 There shall be no signs other than those permitted by these regulations.

03.06.007 The required residential off-street parking shall be maintained.

03.06.008 No vehicles or trailers (including commercial pick-up trucks and vans) or construction and other equipment, except those normally incidental to residential use, shall be kept on the site.

03.07.000 Public Street Standards

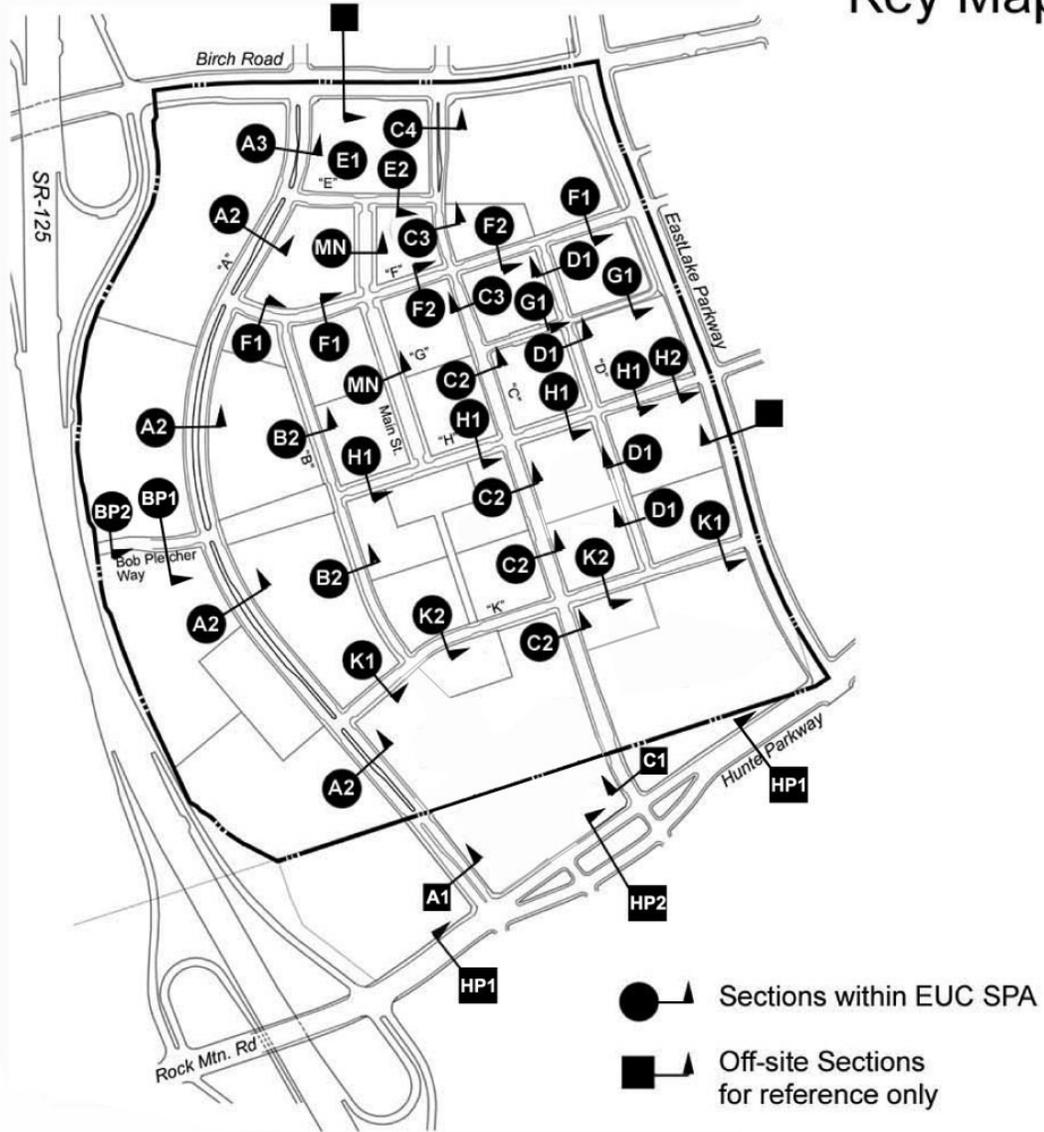
03.07.001 Street standards for the *EUC* shall comply with the standards specified herein as Exhibits III-12 through III-26 (Street Sections), the *EUC SPA* Plan and the tentative subdivision map. For any standard not specified therein, the standards in the *CVMC*, Title 12, Streets and Sidewalks shall apply. Minor variations may occur during the design process. In the event of any internal conflict between the *EUC SPA* and the *FBC*, the *FBC* shall apply. Any internal conflict between the *FBC* and the tentative subdivision map, the tentative map shall apply.

03.07.002 Other Public Streetscape Standards

In addition to the street assemblies identified above, the *EUC* includes special street improvements which are essential to achieving the *EUC* design character but which vary from the standardized improvements used throughout the City of Chula Vista. The following *EUC* street improvement standards shall be implemented:

- a. Neck down Standards: All internal intersections will be designed to enhance pedestrian movement by using special neck down intersection standards whenever vehicular turning movements allow. The standards for neck down intersections is depicted on Exhibit III-28 (Street Assembly).
- b. Encroachments: All above and below ground encroachments into the Public right-of-way require an encroachment permit. The application for Design Review, or other applicable permit, shall provide evidence to the satisfaction of the Zoning Administrator and City Engineer that the public safety will not be jeopardized, or can be mitigated, by the encroachment. These minor encroachments include:
 - i. Encroachments for entry features and stoops shall be subject to the standards shown on Exhibit III-27 (Street Assembly), and example photographs indicated in the building typologies.
 - ii. Encroachments for balconies and awnings or roofed colonnades above the first floor shall be subject to the standards shown on Exhibit III-27 (Street Assembly).
 - iii. Encroachments for first floor awnings: An awning adjacent to a commercial use on the first floor may extend into the adjacent pedestrian corridor, subject to Design Review approval.
 - iv. Encroachments for over-street pedestrian bridges or full pedestrian corridor coverage with a colonnade: These are permitted subject to approval of the Director of Engineering and Planning and Building Director.
 - v. Encroachments for major entry signing above a street's right-of-way. These shall be permitted subject to approval of the sign by the Director of Planning and Building.
 - vi. Encroachments to extend an occupied structure above the first floor, over the pedestrian corridor, may be permitted, subject to Design Review approval.
 - vii. Encroachments for cafes, outdoor displays and sales.
 - viii. Encroachments of the type similar to, but inconsistent with the dimensions indicated, those specified on Exhibit III-27 (Street Assembly), shall be subject to approval by the Zoning Administrator.
 - ix. Other encroachments that enhance the character and intent of the *EUC*, subject to approval of the Zoning Administrator.

Street Sections Key Map

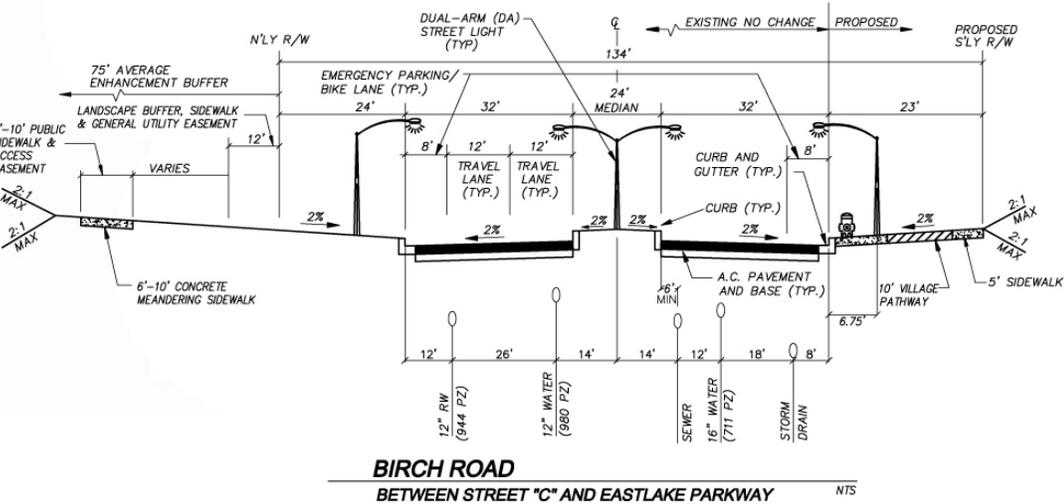
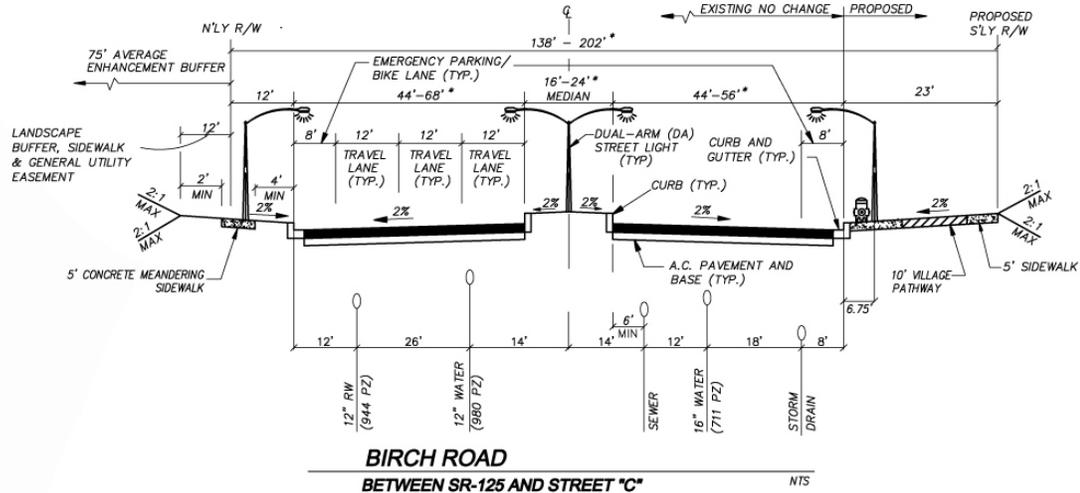


Eastern Urban Center OTAY RANCH

Source: PDC
Urban Design: RTKL
Cinti Land Planning
San Diego, CA (619) 523-7488

Exhibit III-12

Street Sections Birch Road

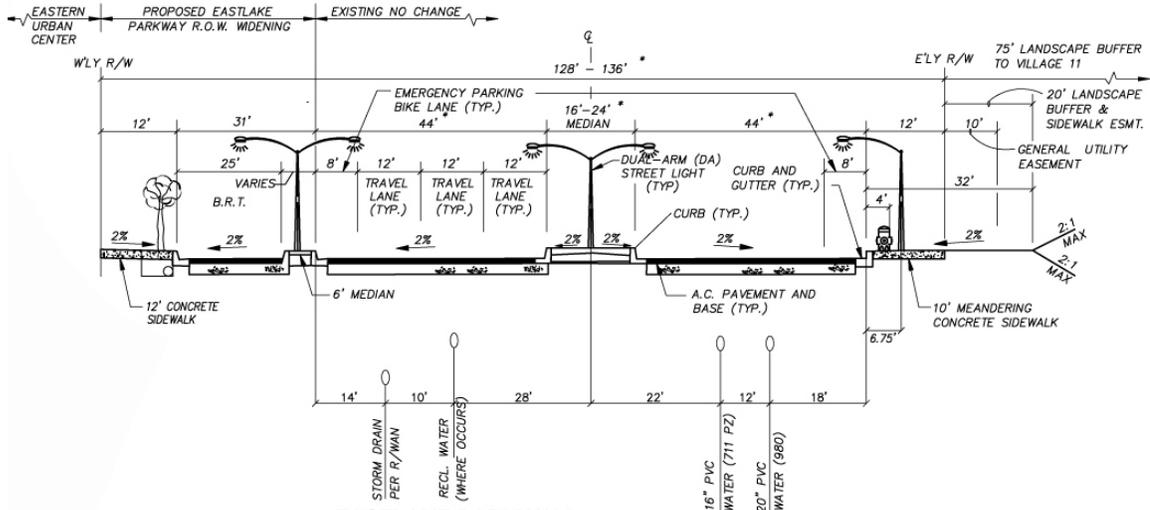


Source: PDC
Urban Design: RTKL
Cinti Land Planning
San Diego, CA (619) 223-7408

Exhibit III-13

Street Sections

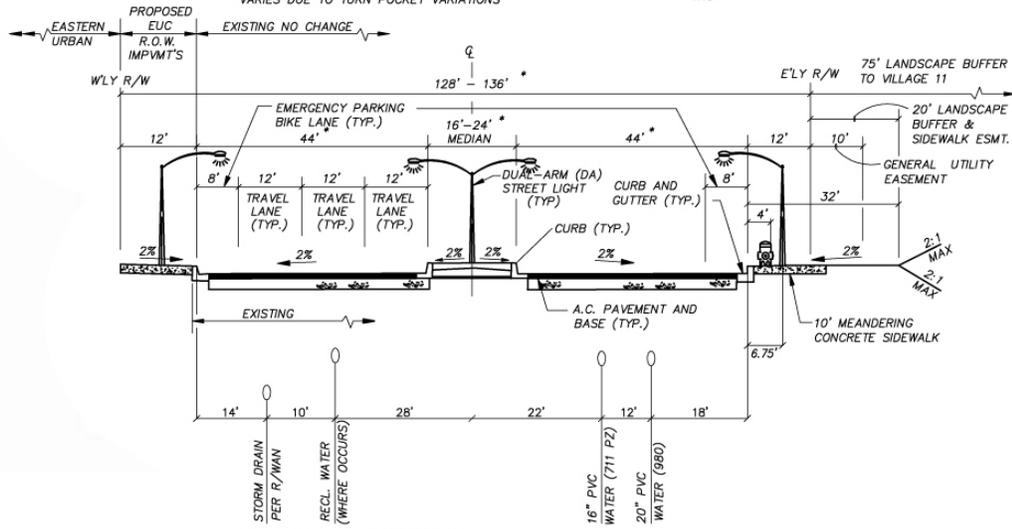
Eastlake Parkway



EASTLAKE PARKWAY

* VARIES DUE TO TURN POCKET VARIATIONS

NTS



EASTLAKE PARKWAY

* VARIES DUE TO TURN POCKET VARIATIONS

NTS



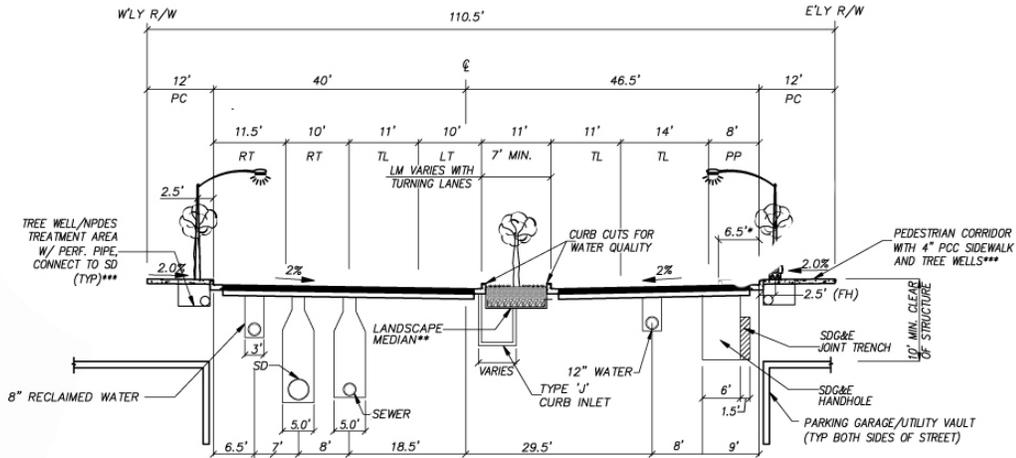
Eastern Urban Center
OTAY RANCH

Source: PDC
Urban Design: RTKL
Cinti Land Planning
San Diego, CA (619) 223-7408

Exhibit III-14

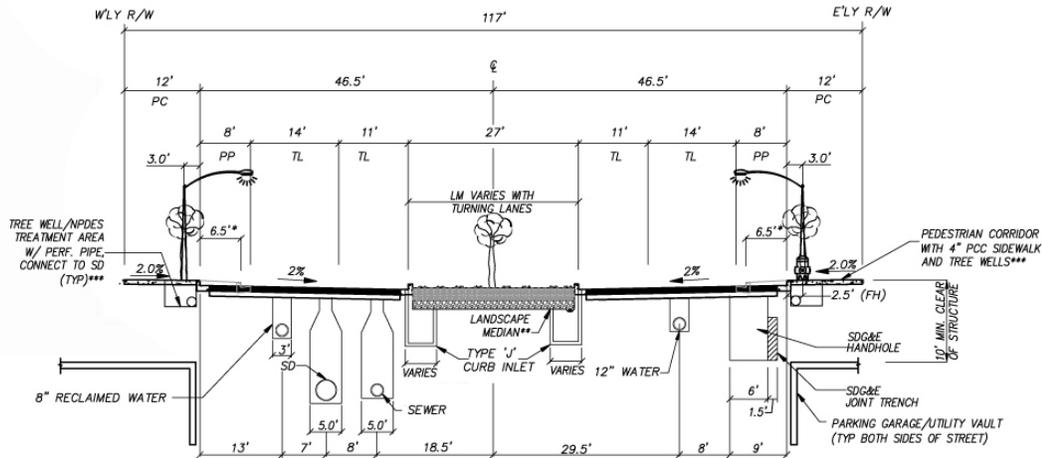
Street Sections

Streets "A1" & "A2"



A-1 — **STREET "A"**
4 LANE MAJOR - (25 MPH) NTS
PUBLIC STREET

* _ AT CURB POP-OUTS
 ** FOR LANDSCAPE MEDIAN DETAIL, SEE SECTION ON SHEET 3
 *** WIDTH OF TREE WELLS PER LANDSCAPE PLAN, DEPTH OF GARAGE TO BE DETERMINED AT TIME OF THE IMPROVEMENT PLAN REVIEW



A-2 — **STREET "A"**
4 LANE MAJOR - (25 MPH) NTS
PUBLIC STREET

* _ CURB AT POP-OUT
 ** FOR LANDSCAPE MEDIAN DETAIL, SEE SECTION ON SHEET 3
 *** WIDTH OF TREE WELLS PER LANDSCAPE PLAN, DEPTH OF GARAGE TO BE DETERMINED AT TIME OF THE IMPROVEMENT PLAN REVIEW

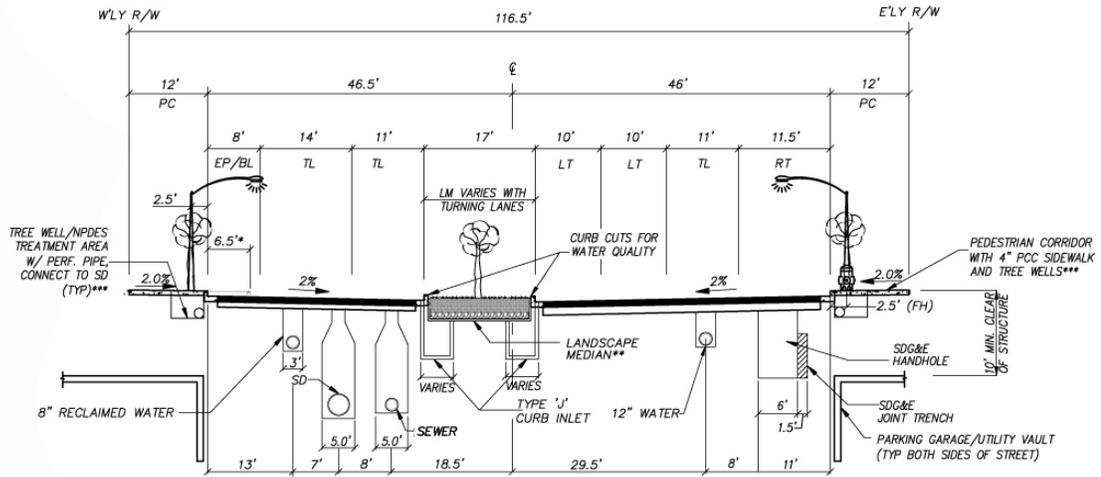


Source: PDC
 Urban Design: RTKL
 Cinti Land Planning
 San Diego, CA (619) 223-7408

Exhibit III-16

Street Sections

Street "A-3"



A-3 **STREET "A"**
4 LANE MAJOR - (25 MPH) NTS
PUBLIC STREET

* - AT CURB POP-OUTS
 ** FOR LANDSCAPE MEDIAN DETAIL, SEE SECTION ON SHEET 3
 *** WIDTH OF TREE WELLS PER LANDSCAPE PLAN, DEPTH OF GARAGE TO BE DETERMINED AT TIME OF THE IMPROVEMENT PLAN REVIEW

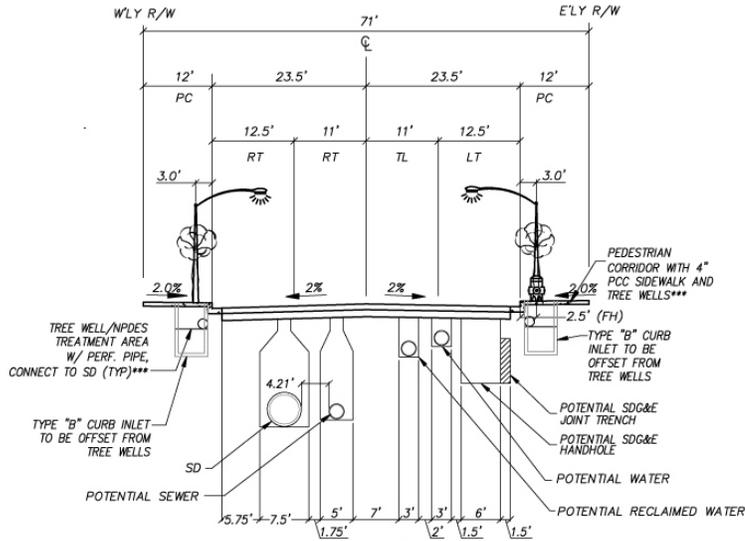


Source: PDC
 Urban Design: RTKL
 Cinti Land Planning
 San Diego, CA (619) 223-7408

Exhibit III-17

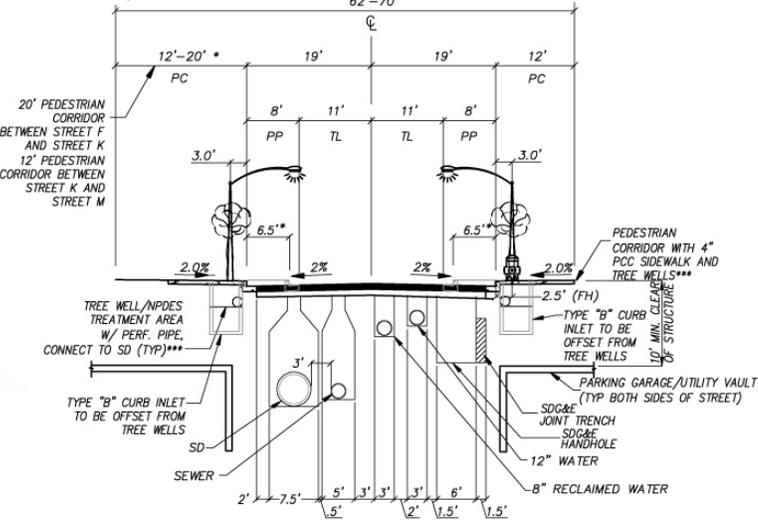
Street Sections

Streets "B-1" & "B-2"



B-1
STREET "B"
 2 LANE CORE - (25 MPH)
 PUBLIC STREET NTS

*** WIDTH OF TREE WELLS PER LANDSCAPE PLAN, DEPTH OF GARAGE TO BE DETERMINED AT TIME OF THE IMPROVEMENT PLAN REVIEW



B-2
STREET "B"
 2 LANE CORE - (25 MPH)
 PUBLIC STREET NTS

*** WIDTH OF TREE WELLS PER LANDSCAPE PLAN, DEPTH OF GARAGE TO BE DETERMINED AT TIME OF THE IMPROVEMENT PLAN REVIEW

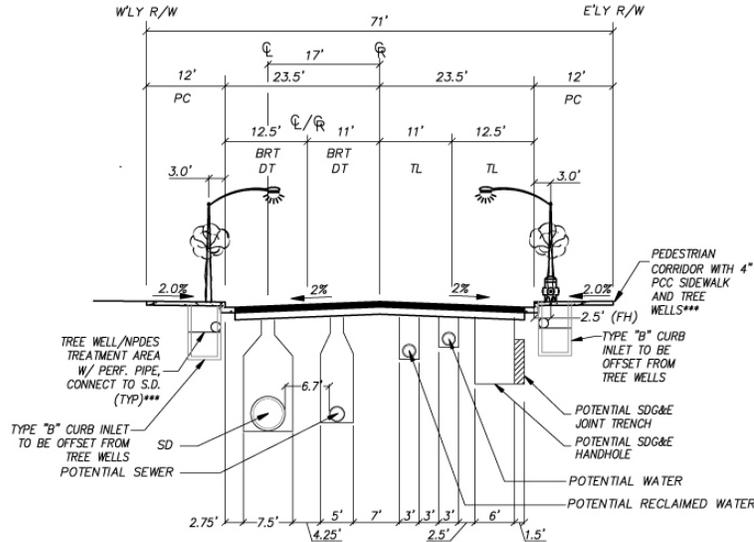


Source: PDC
 Urban Design: RTKL
 Cinti Land Planning
 San Diego, CA (619) 233-7408

Exhibit III-18

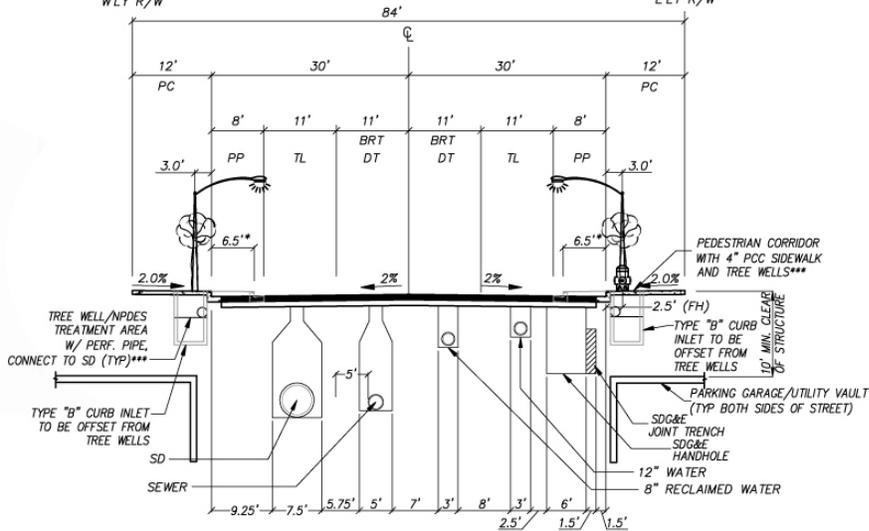
Street Sections

Streets "C-1" & "C-2"



C-1 **STREET "C"**
 2 LANE CORE - (25 MPH) NTS
 PUBLIC STREET

*** WIDTH OF TREE WELLS PER LANDSCAPE PLAN, DEPTH OF GARAGE TO BE DETERMINED AT TIME OF THE IMPROVEMENT PLAN REVIEW
 WLY R/W



C-2 **STREET "C"**
 2 LANE CORE - (25 MPH) NTS
 PUBLIC STREET

*** WIDTH OF TREE WELLS PER LANDSCAPE PLAN, DEPTH OF GARAGE TO BE DETERMINED AT TIME OF THE IMPROVEMENT PLAN REVIEW



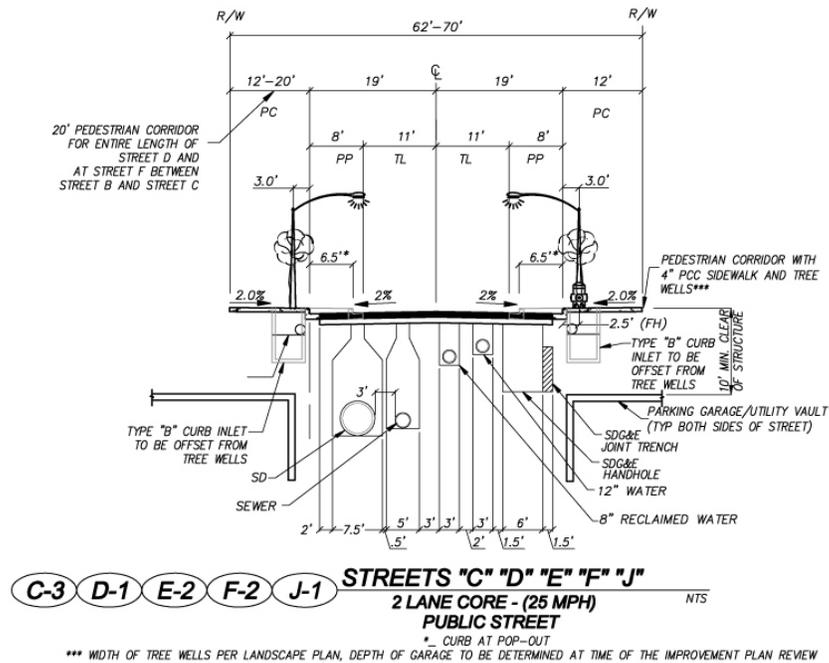
Eastern Urban Center
OTAY RANCH

Source: PDC
 Urban Design: RTKL
 Cinti Land Planning
 San Diego, CA (619) 223-7408

Exhibit III-19

Street Sections

Streets "C-3, D-1, E-2, F-2, J-1"



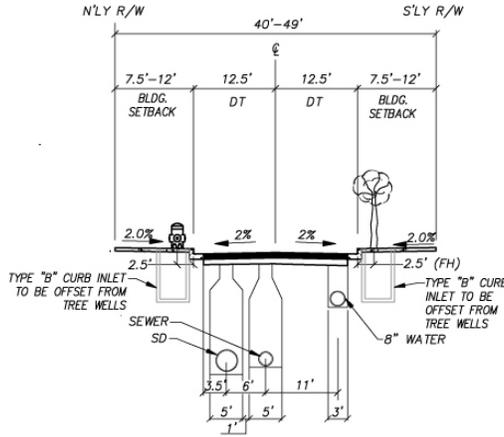
Eastern Urban Center
OTAY RANCH

Source: PDC
 Urban Design: RTKL
 Cinti Land Planning
San Diego, CA (619) 223-7408

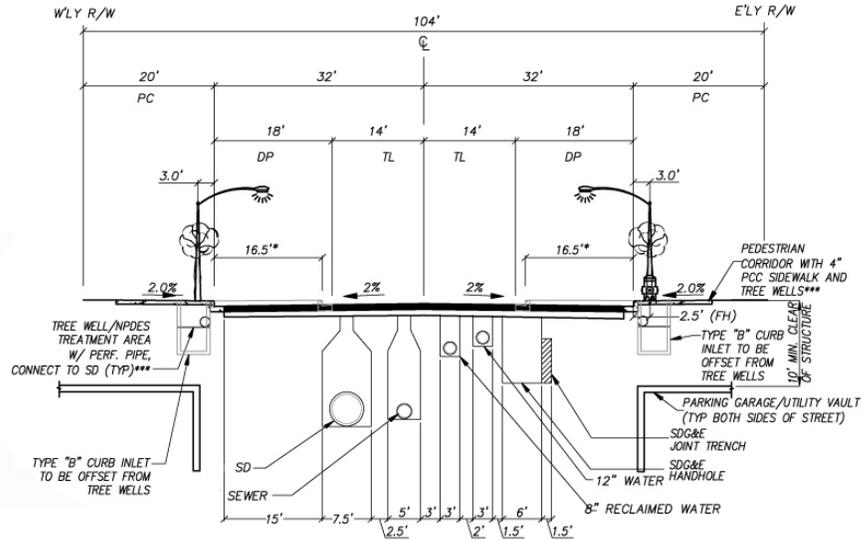
Exhibit III-20

Street Sections

Streets "G-1" & "MAIN"



STREET "G"
G-1 2-LANE TRANSIT GUIDEWAY
 PUBLIC STREET NTS



MAIN STREET
MN 2 LANE CORE - (25 MPH)
 PUBLIC STREET NTS

*** WIDTH OF TREE WELLS PER LANDSCAPE PLAN, DEPTH OF GARAGE TO BE DETERMINED AT TIME OF THE IMPROVEMENT PLAN REVIEW
 * CURB AT POP-OUT



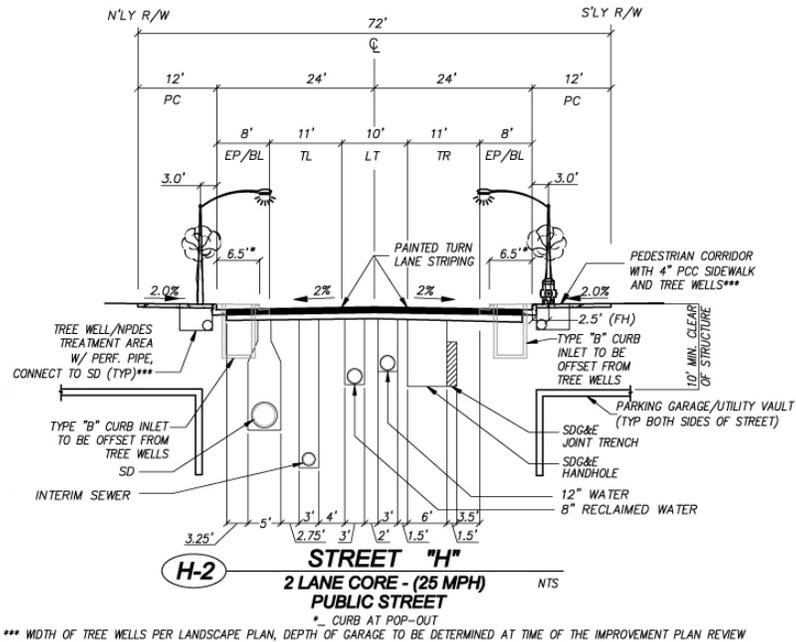
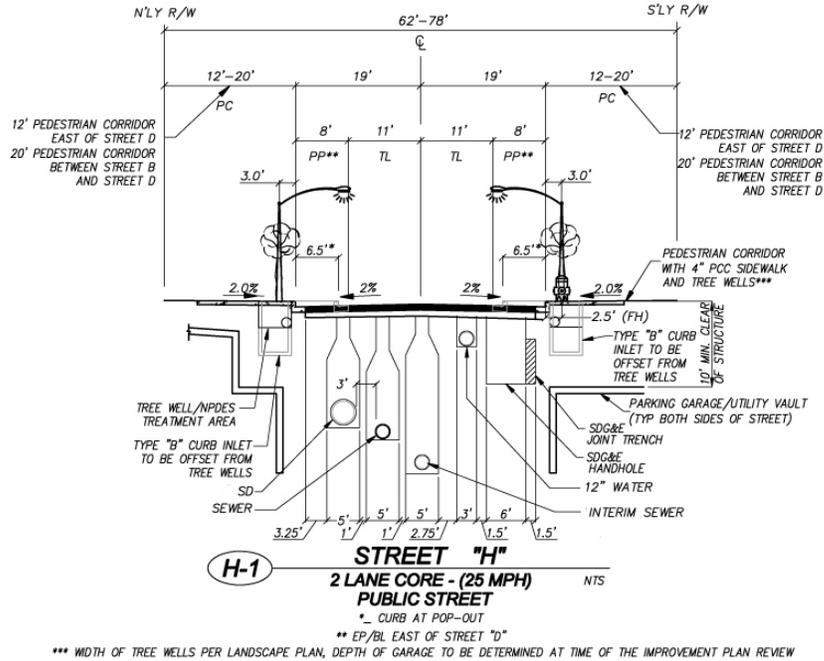
Eastern Urban Center
OTAY RANCH

Source: PDC
 Urban Design: RTKL
 Cinti Land Planning
 San Diego, CA (619) 223-7408

Exhibit III-22

Street Sections

Streets "H-1" & "H-2"



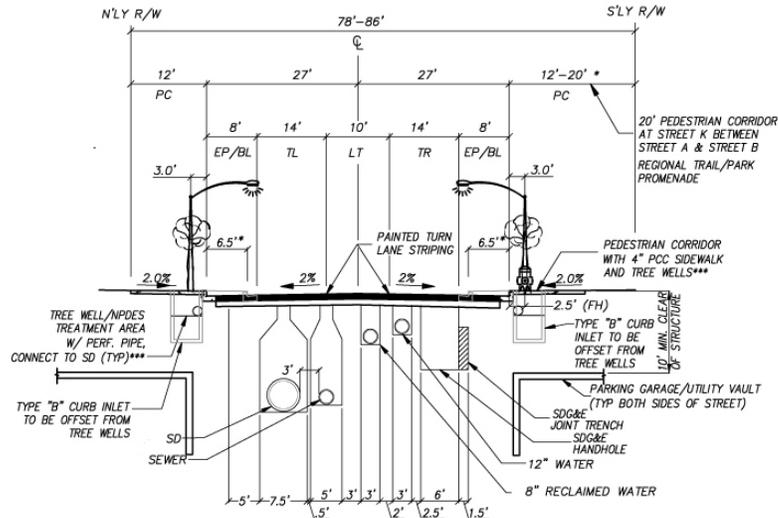
Eastern Urban Center
OTAY RANCH

Source: PDC
 Urban Design: RTKL
 Cintil Land Planning
 San Diego, CA (619) 223-7408

Exhibit III-23

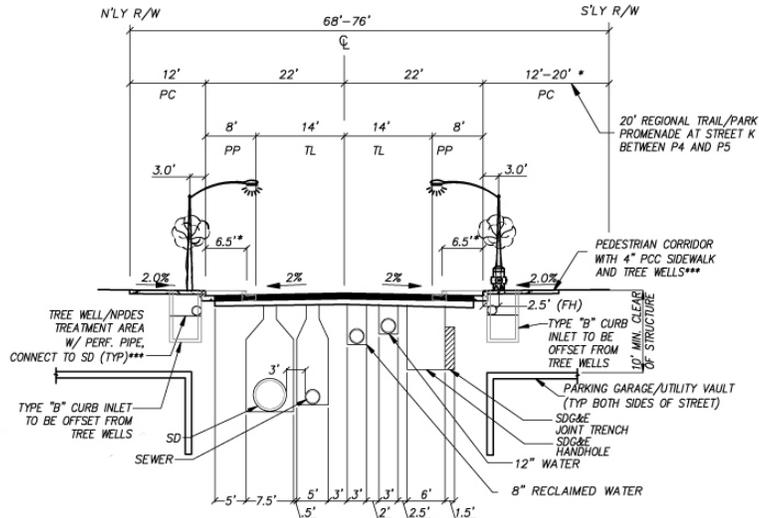
Street Sections

Streets "K-1" & "K-2"



K-1
STREET "K"
 2 LANE CORE - (25 MPH)
 PUBLIC STREET

*** WIDTH OF TREE WELLS PER LANDSCAPE PLAN, DEPTH OF GARAGE TO BE DETERMINED AT TIME OF THE IMPROVEMENT PLAN REVIEW



K-2
STREET "K"
 2 LANE CORE - (25 MPH)
 PUBLIC STREET

*** WIDTH OF TREE WELLS PER LANDSCAPE PLAN, DEPTH OF GARAGE TO BE DETERMINED AT TIME OF THE IMPROVEMENT PLAN REVIEW



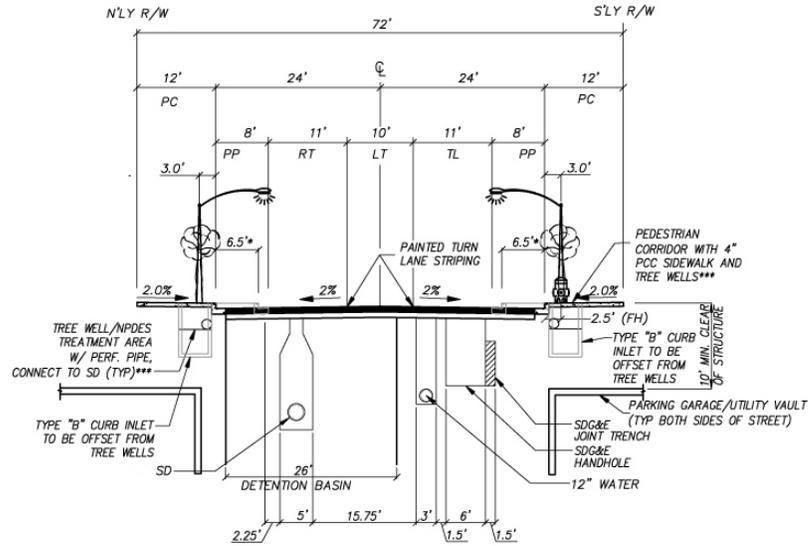
Eastern Urban Center
OTAY RANCH

Source: PDC
 Urban Design: RTKL
 Cinti Land Planning
 San Diego, CA (619) 223-7408

Exhibit III-24

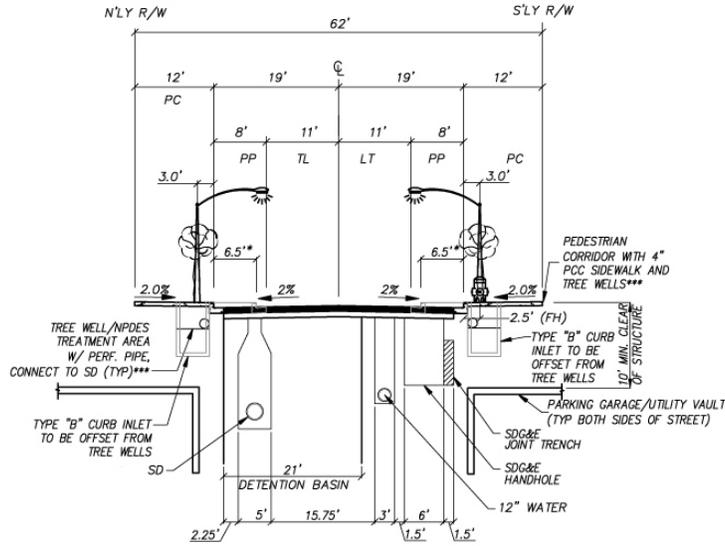
Street Sections

Streets "M-1" & "M-2"



M-1 **STREET "M"**
2 LANE CORE - (25 MPH) NTS
PUBLIC STREET

*** WIDTH OF TREE WELLS PER LANDSCAPE PLAN, DEPTH TO BE DETERMINED AT TIME OF THE IMPROVEMENT PLAN REVIEW



M-2 **STREET "M"**
2 LANE CORE - (25 MPH) NTS
PUBLIC STREET

*** WIDTH OF TREE WELLS PER LANDSCAPE PLAN, DEPTH OF GARAGE TO BE DETERMINED AT TIME OF THE IMPROVEMENT PLAN REVIEW



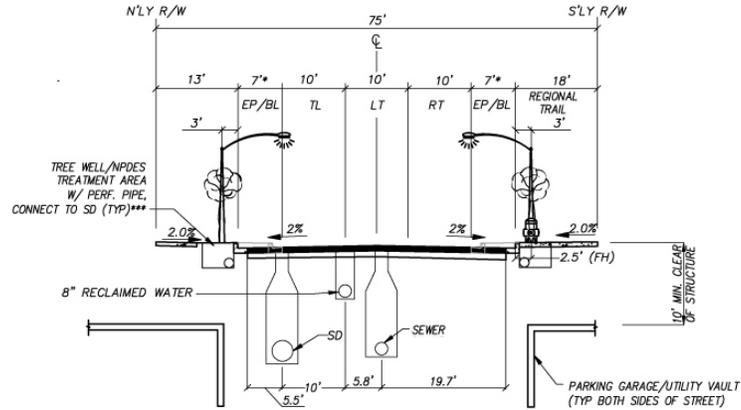
Eastern Urban Center
OTAY RANCH

Source: PDC
 Urban Design: RTKL
 Cinti Land Planning
 San Diego, CA (619) 223-7400

Exhibit III-25

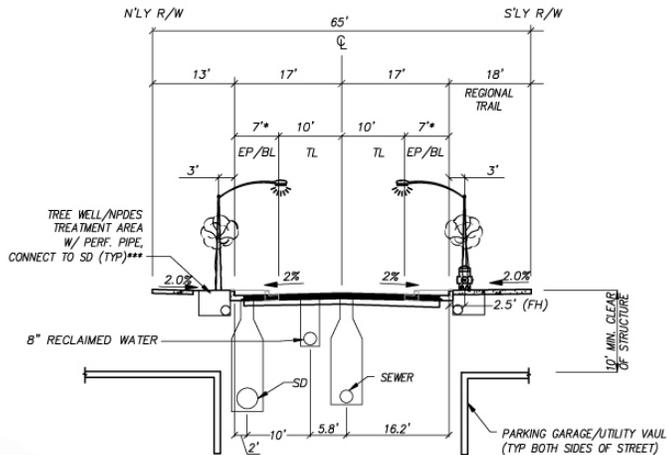
Street Sections

Bob Pletcher Way



BP1 **BOB PLETCHER WAY**
 2 LANE SECONDARY VILLAGE ENTRY - (25 MPH) ^{NTS}
 PUBLIC STREET

* - CURB AT POP-OUT
 *** WIDTH OF TREE WELLS PER LANDSCAPE PLAN, DEPTH OF GARAGE TO BE DETERMINED AT TIME OF THE IMPROVEMENT PLAN REVIEW



BP2 **BOB PLETCHER WAY**
 2 LANE SECONDARY VILLAGE ENTRY - (25 MPH) ^{NTS}
 PUBLIC STREET

* - CURB AT POP-OUT
 *** WIDTH OF TREE WELLS PER LANDSCAPE PLAN, DEPTH OF GARAGE TO BE DETERMINED AT TIME OF THE IMPROVEMENT PLAN REVIEW

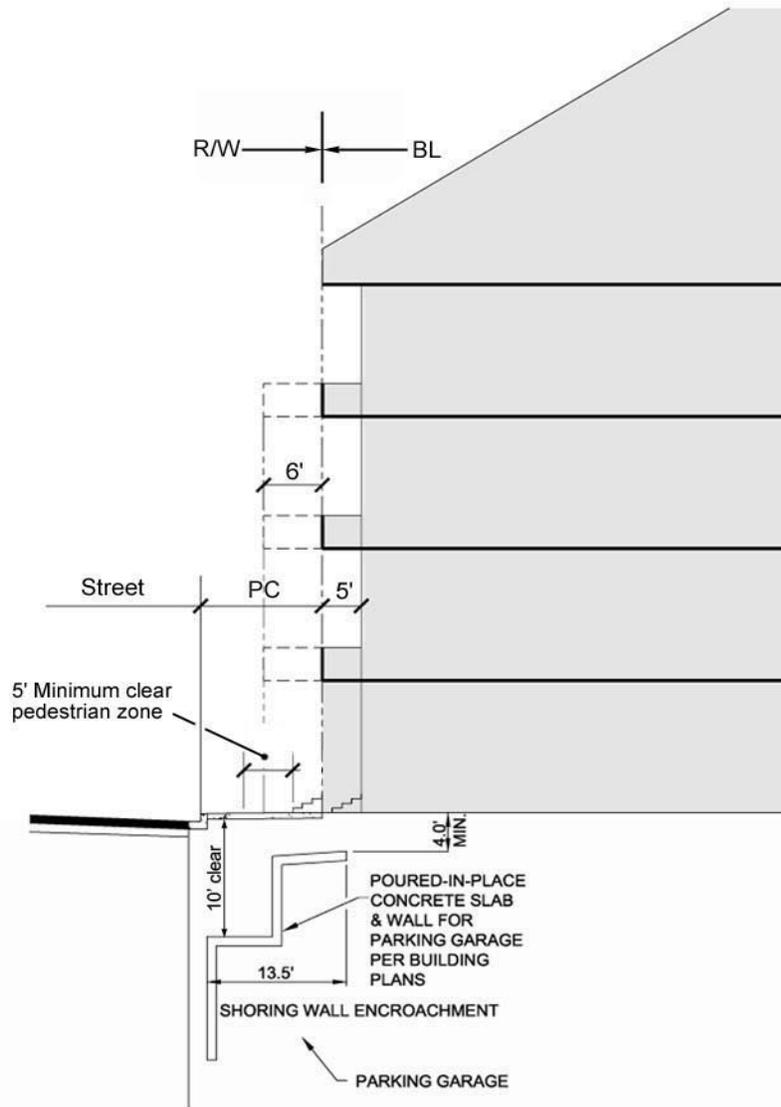


Eastern Urban Center
OTAY RANCH

Source: PDC
 Urban Design: RTKL
 Cinti Land Planning
 San Diego, CA (619) 223-7408

Exhibit III-26

Encroachments



Note: Refer to text for additional encroachment standards and setback provisions.

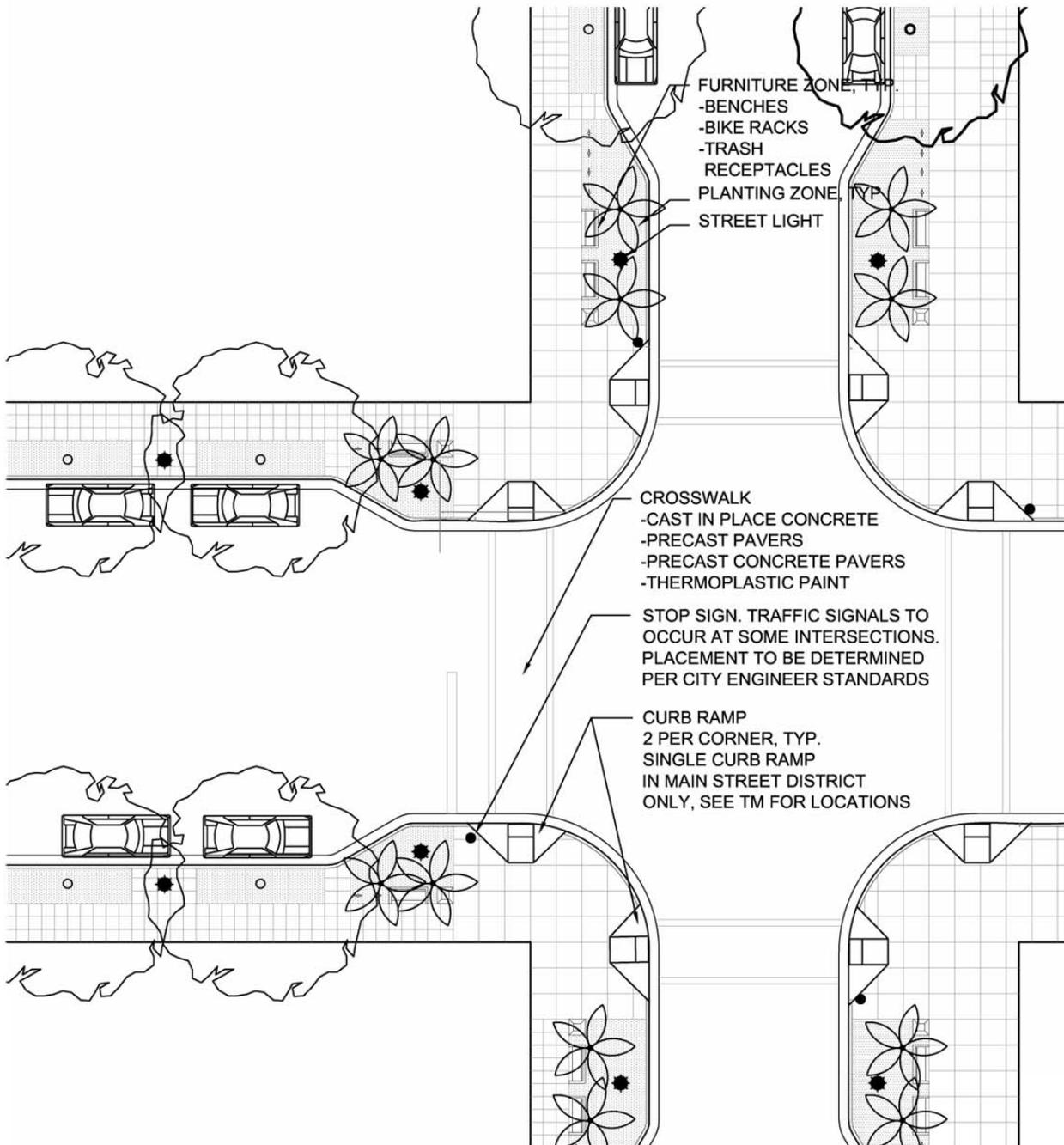


Eastern Urban Center OTAY RANCH



Exhibit III-27

Typical “Neck Down”



Eastern Urban Center OTAY RANCH

Source: PDC
 Urban Design: RTKL
 Cinti Land Planning
 San Diego, CA (619) 223-7468

Exhibit III-28

03.07.003 Access Management:

The grid street system within the *EUC* is formed by local streets extended into the project area from the access points on the surrounding arterial streets. In most cases these access points have been previously established by surrounding development projects. Primary access is provided by two intersections on Birch Road and three intersections on Eastlake Parkway. There will be two or three intersections on Hunte Parkway depending on the final roadway classification. A secondary access point is Bob Pletcher Way which connects under SR 125 to Village 7. Access is also provided from a right in access only and a right in/right out access on Birch Road. Any additional access onto the arterial streets will be evaluated on a case by case basis.

The internal grid street system provides multiple routes to destinations within the *EUC*. However, different streets within the grid street system function in different ways and perform differently within the overall urban design plan. As a result, an access management policy is established to provide guidance on how vehicular access to individual development blocks will be managed so as to be consistent with the intended function and role of each individual street segment.

Access Management Guidelines

All of the internal public streets within the *EUC* have been classified into one of five access management categories as illustrated on Exhibit III-29 (Access Management Plan). These categories are as follows:

a. No Access - Perimeter Streets

The *EUC* perimeter streets are Birch Road on the north, Eastlake Parkway on the east, and a very short segment of Hunte parkway on the south. No vehicular access into the *EUC* from these perimeter roads is permitted except at the locations noted on Exhibit III-29. On Birch Road these access points are the intersections of Birch Road with A and C Streets, a right-in access point between the SR-125 ramps and A Street and a right-in/right-out access point between A and C Streets. On Eastlake Parkway the permitted access points are the intersections of Eastlake parkway and F, H, and K Streets. On Hunte parkway there is no access from the very short segment *EUC* frontage on Hunte parkway immediately west of the Hunte Parkway/Eastlake parkway intersection. Access to Hunte parkway is permitted by the southerly extension of A, B, and C Streets as shown on Exhibit III-29.

b. No Access - Interior Streets:

These street segments are at major entry points to the *EUC* or secondary entry points (Bob Pletcher Way) where the graded topography produces slope banks on either

side of the roadway. Also included are the street segments comprising the Main Street in the northcentral portion of the *EUC*.

For these street segments, no curb cuts will typically be permitted to provide access to the adjacent lots on either side of this roadway segment. Access to adjacent lots will be provided from other *EUC* streets. Should access be desired by an individual development project this will need to be approved by both the *master developer* and by the City Engineer. This proposed access point will need to be supported by a traffic study that describes in detail the configuration of the access point and how the access works considering the surrounding traffic circulation conditions.

c. Limited Access

These street segments include the secondary entry point to the *EUC* on Birch Road at Street C and two secondary entry points on Eastlake Parkway at Street F and Street K. For these street segments, right in/right out access to the adjacent lots is permitted. For the secondary *EUC* entrances, the location of these right in/right out access points may be limited by the vehicular stacking and other requirements near the signalized intersections on the arterial streets.

Another limited access street segment is the portion of Street A between Street E and Street M. Access to the adjacent lots and intersecting streets is at the eight intersections shown on Exhibit III-29 (Access Management Plan). Street A includes a 27 foot landscaped median that can accommodate protected single and double left turn pockets. These eight intersections will permit left turns where protected turn pockets are provided. For this segment of Street A any right in/right out access will need to be recommended by the *master developer* and approved by the City Engineer.

The *EUC* fire station is located at the southeast corner of the intersection of A and F Streets. Emergency vehicles will exit the station onto A Street and can either turn north or turn south via the break in the A Street median designated for emergency vehicle use only. A right-in/right-out access point on is also permitted on A Street into the fire station employee and visitor parking lot.

d. BRT Route Managed Access

These street segments include the portions of Street C that have the BRT Route in the center two lanes of the 4-lane local street. It also includes the portion of Street G that connects Street C with the dedicated transit lane portion of the BRT route located along Eastlake Parkway. For these portions of Street C and Street G

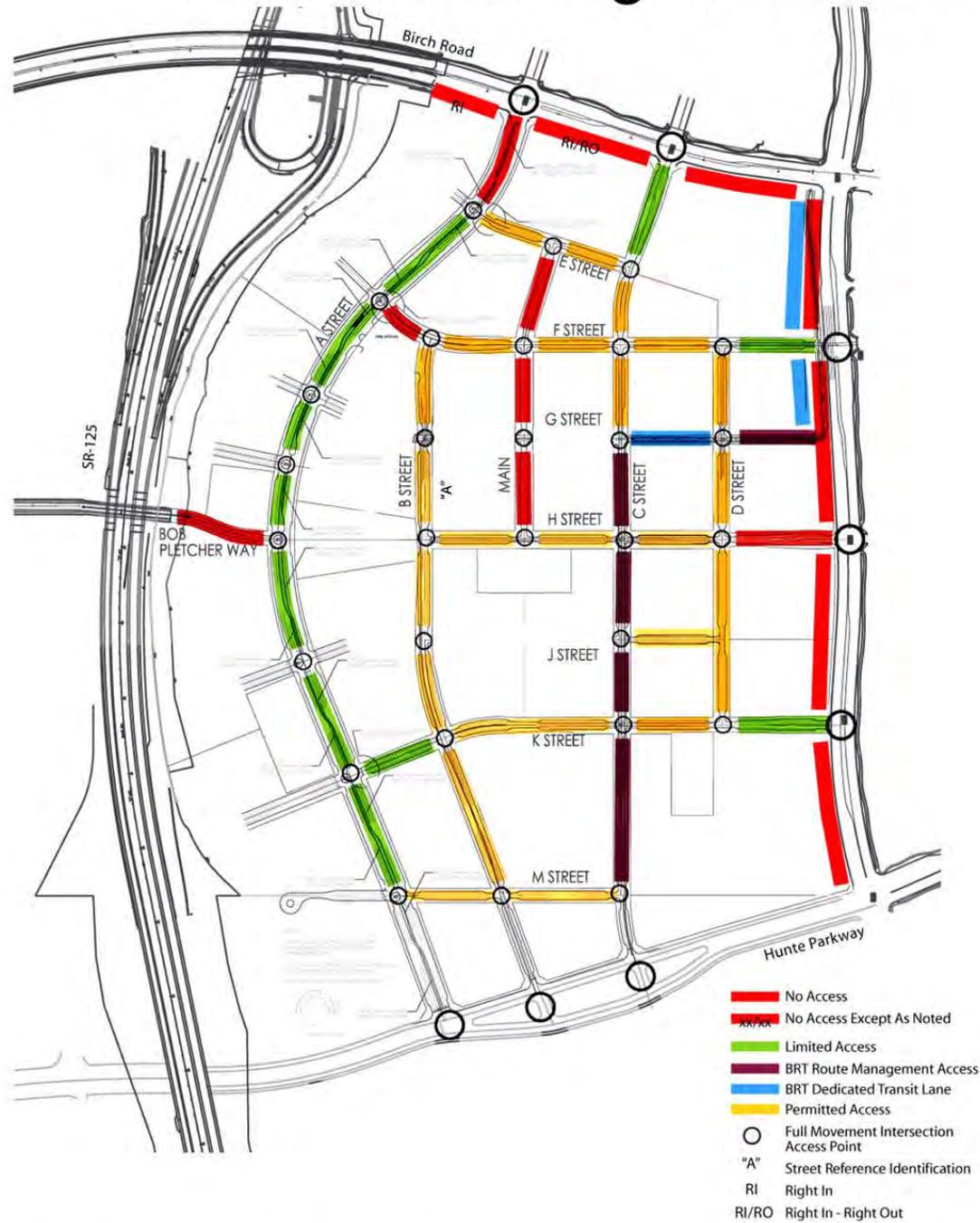
(between “D” street and Eastlake Parkway) curb cuts to provide access to the adjacent blocks are permitted.

Although access is permitted from the BRT Route into adjacent lots, it is recommended that where access can be provided from streets categorized as “Permitted Access” site planning should focus on primary access from those streets and limit curb cuts on the BRT Route. In many cases, however, blocks will require some access points on the BRT Route

e. Permitted Access

These street segments are the balance of the public street system within the *EUC*. They comprise portions of Street B, C, D, E, F, H, J, K, and M. For these street segments, curb cuts are permitted to provide access to the adjacent lots. As noted above and where a block is bounded by the streets with the BRT Route and streets categorized as permitted access the preferred streets for primary access to the block are those designated “Permitted Access.”

Access Management Plan



 **Eastern Urban Center**
CITY OF CHULA VISTA **OTAY RANCH**

Source: EDAW
Urban Design: RTKL
Cinti Land Planning
San Diego, CA (619) 233-7400

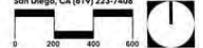


Exhibit III-29

03.08.000 Standards for Pedestrian Corridors, Sidewalks and Trails

03.08.001 Purpose and Intent: The pedestrian system satisfies three primary objectives:

- a. To connect internal uses and public places both within and beyond the *EUC*;
- b. To provide a system that connects public places to encourage social interaction among the residents; and,
- c. To promote the ‘park once’ parking objective.

The orientation is a primary component of all streets and extends beyond simply providing pedestrian access routes through the districts. Facilities, structures and the primary circulation routes are specifically organized, scaled and designed to encourage pedestrian circulation throughout the project. The overall pedestrian system is depicted in Exhibit III-32 (Pedestrian Corridors) and the Regional Trails are shown on Exhibit III-31 (Regional Trails). The primary components of the pedestrian system are described below.

03.08.002 Standards: Except as otherwise specified herein, the standards in the CVMC, Title 12, Streets and Sidewalks shall apply.

03.08.003 Primary Pedestrian Elements

- a. The primary pedestrian elements are the Regional Trail and the Village Pathway routes which connect through the *EUC* to destinations throughout Otay Ranch. The design concept for these community trail systems is to interconnect the surrounding community to the various components of the *EUC*. The proposed distinctive design character of these trail systems is intended to both compatibly reflect the district it is located within and be coordinated with the proposed streetscape design character it is located within. As the Regional Trail passes from its western anchor at Wolf Canyon to its eastern anchor at Salt Creek, the trail is aligned through the *EUC* to create a series of six interconnected experiences. The trails are shown herein on Exhibits III-33 - III-34(Regional Trails). Where the Regional Trail coincides with other pedestrian corridors, the standards for the Regional Trail shall take precedence.
 - i. *Bob Pletcher Way*: the western *EUC* gateway emerging from the Route 125 underpass.
 - i. *Civic Core*: the main civic events plaza/paseo of the *EUC*.
 - ii. *Main Street*: widened pedestrian corridors with café zones create a central activated spine in the *EUC*, which has the Town Square park at it’s southern

terminus.

- iii. *Urban Neighborhood*: the vibrant mixed-use residential streetscapes of the *EUC*.
- iv. *Neighborhood Park*: flanking the southeastern neighborhood focal point of the *EUC*.
- v. *Residential Paseo*: linking the pedestrian bridge over East Lake Parkway that forms the trail system’s eastern gateway to the *EUC*.
- vi. *Park Promenade*: Connects the Town Square at the end of Main Street to the park in the South Central Residential District.
- vii. *Other Pedestrian Elements*: Other pedestrian elements are the pedestrian routes which are not located adjacent to a street. The design concept for these routes is that of a “paseo” or “mew street” Exhibit III-30 (Mew Street). The design concepts for paseo routes within the *EUC* are illustrated in Exhibit III-42 (Pedestrian Corridors). These paseos may be for both pedestrian and vehicular use, subject to appropriate design treatments to ensure pedestrian usability and safety.

Within Districts 1,4 and 8, Exhibit III-32 and III-40 (Pedestrian Corridors - Widths and Locations) a jogging path is proposed, which when combined with other pedestrian paths will form a loop system for jogging. The site planning of these districts shall include this path, which may either be an uninterrupted external alignment or part of an internal system that integrates plazas and buildings. There shall be a seamless integration of this path between ownership parcels. The public use of the path shall be assured by either a public lot or easement.

- b. The following elements shall be utilized to establish continuity along the length of the Regional Trail pedestrian route:
 - i. A continuous alignment through the *EUC* with a minimum clear width of ten (10') feet.
 - ii. Interconnects the various components of the *EUC* (e.g., provides a comprehensive 'snapshot' of the *EUC*).
 - iii. Way-finding Markers highlighting key direction changes (pavement medallions, wall- or post-mounted placards, information kiosk, etc.).
 - iv. Interpretive Trail Markers tying the trail to its surrounding community

context and community heritage (*e.g.*, commemorative memorial, scenic view point, place-making marker, *etc.*).

- v. Common Design Elements (*e.g.*, coordinated 'kit of parts')Common Design Elements shall include:
 - (1) Continuity of Alignment & Width.
 - (2) Special Paving Material or Color with varied site applications (*e.g.*, 'whole trail width' in Civic Plaza, 'linear centered' in urban streetscape, 'linear off-set' flanking park, 'banding' in urban neighborhood applications).
 - (3) Way-finding & Commemorative Art Marks (*e.g.*, coordinated family of pavement medallions, wall or pole-mounted placards, or signage & banners).
 - (4) Distinctive Pedestrian Lighting, where needed (*e.g.*, pedestrian bollard lights, pole lights with or without banners, appropriately coordinated to compliment the setting and avoid clutter).
 - (5) Coordinated Street Furniture (*e.g.*, coordinated family of benches, bike racks, *etc.*).
 - (6) Coordinated Public Art placed in highly visible areas, consistent with the City of Chula Vista's Arts Master Plan, when adopted. Public art may include functional forms, such as unique paving, street furniture, bicycle racks, and designed fountains. Refer to Exhibit III-78(Public Art), for examples.

- d. The Village Pathway and Regional Trail: The Village Pathway connects the *EUC* to Village Seven to the west and Village Eleven to the east along Birch Road. Exhibits III-33 and III-34 (Urban Landscape Character- Regional Trials) will maintain the ten-foot wide clear pedestrian passage through the *EUC*.

- e. Secondary Pedestrian Elements: Secondary pedestrian elements are the remaining pedestrian routes located adjacent to a street. The design concept for these routes is similar to that of the primary route but with a reduced level of enhancement. The design concept for the secondary pedestrian routes within the *EUC* is illustrated in Exhibits III-35to III-41 Pedestrian Corridor).

Example of a Mews Street



Exhibit III-30

f. Internal Connections (Mew Streets):

A mew street is pedestrian passage way featuring a unique design that also allows vehicular travel as a low speed driveway. These may be located as extensions of “G” and “J” streets through the Main Street district between B Street and C Streets, intersecting Main Street near the center of the retail corridor. The “G” street extension would connect Main Street to the transit station east of C Street. The design of mew streets will utilize techniques that enhance the pedestrian experience and reduce or eliminate daily vehicular traffic. Exhibit III-30 (Example of a Mew Street) indicates a typical mew street. Typical design treatments include:

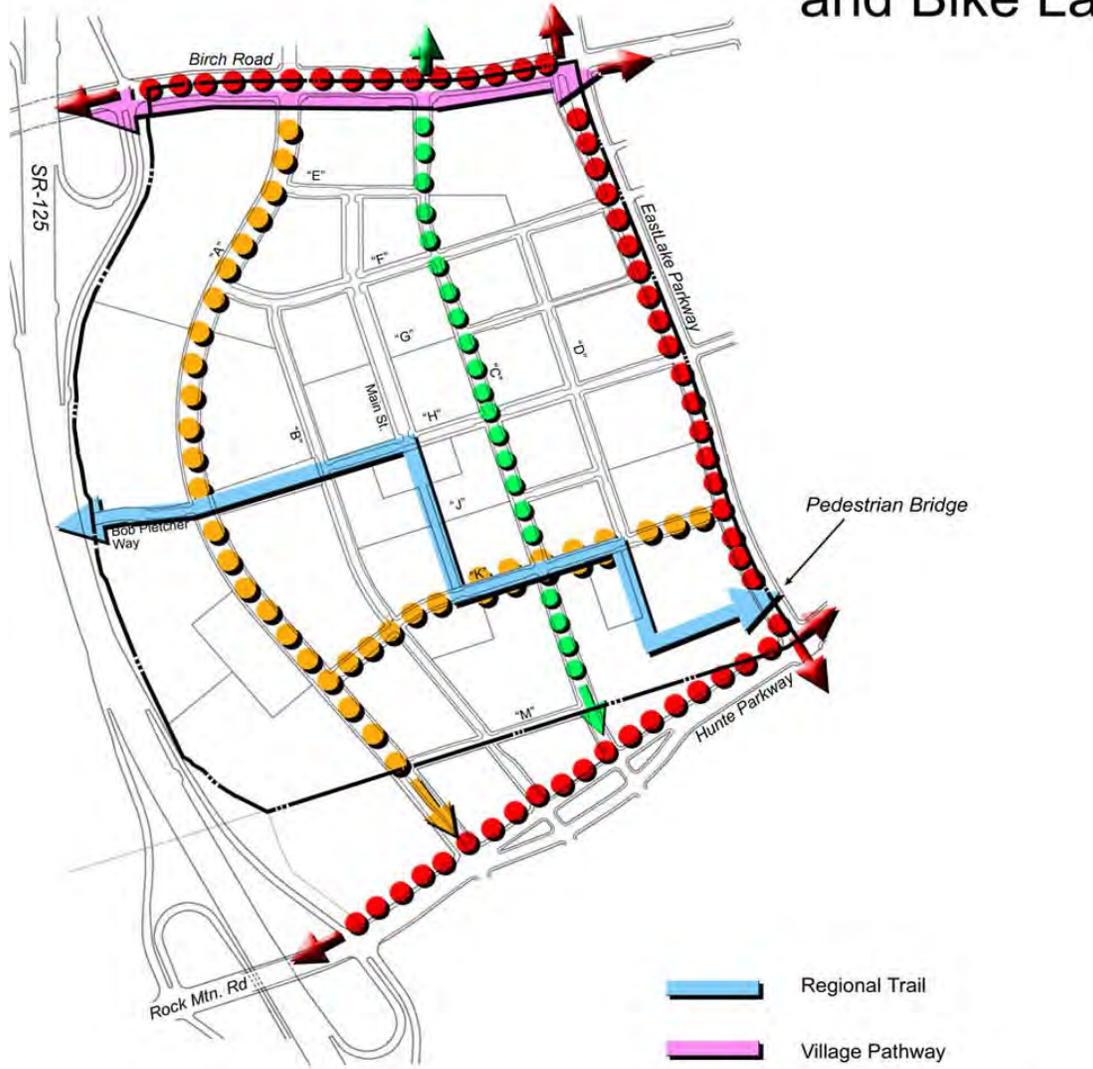
- i. Reduce the distinction between the pedestrian corridor and any remaining vehicular travel way using landscaping, enhanced paving, and modified curb or no curb designs.
- i. Reduce the distinction between the pedestrian corridor and any remaining vehicular travel way using landscaping, enhanced paving, and modified curb

or no curb designs.

- ii. Create a more intimate pedestrian scale by reducing the building face to building face dimension. A minimum 44' building separation should be provided with an overall section width of 44-60 feet. A 22' paved way should be provided or 20 when access is for emergency vehicles only.
- iii. Reduced the scale of parking along a Mew Street. No parking, parking pockets or parking on one side can emphasize the pedestrian character.
- iv. Multiple alignment alternatives are possible although both the "G" and "J" mews must line up with existing intersections on "C" Street. The termini with "B" street may vary due to the lack of intersections along "B" street.
- v. Provide for parking entrances and emergency vehicle access where necessary.

Multiple design solutions for the EUC mews streets are possible and any design must be responsive to both the design and planned land uses of the adjacent buildings, so the final design of these internal connections will be determined during the Design Review process.

Regional Trails and Bike Lanes



-  Regional Trail
-  Village Pathway
-  Inter-Village Connection
-  Regional Bike Lanes (Emergency parking/bike lanes)
-  Bike Route (14' widened travel lane)

Note: Due to low traffic speeds & volumes, streets not indicated as having specific provisions for bike lanes/routes can accommodate bicycle traffic as mixed flow.

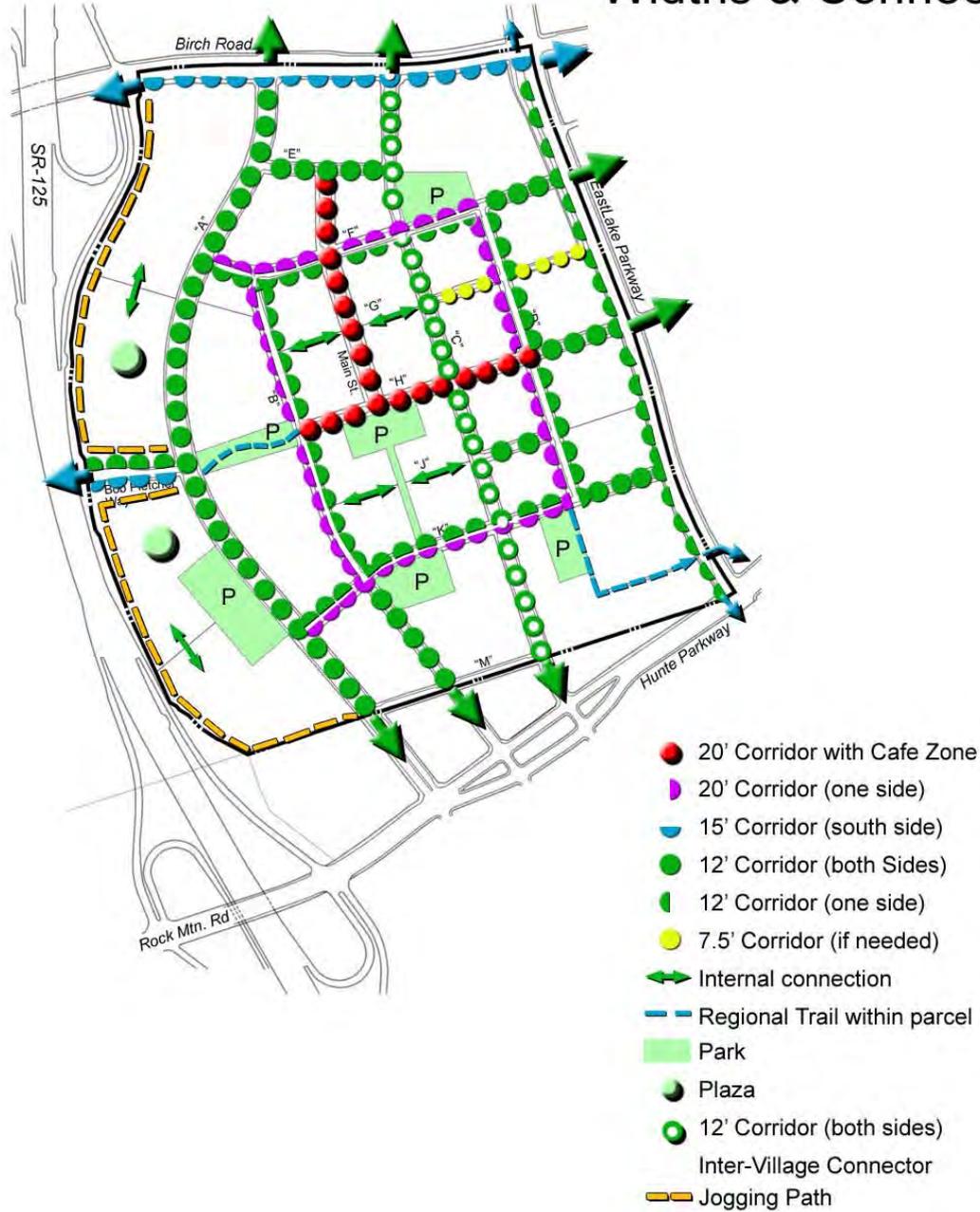


Eastern Urban Center OTAY RANCH



Exhibit III-31

Pedestrian Corridors Widths & Connections



Urban Landscape Character

Regional Trail



Wayfinding / Signing



Medallions

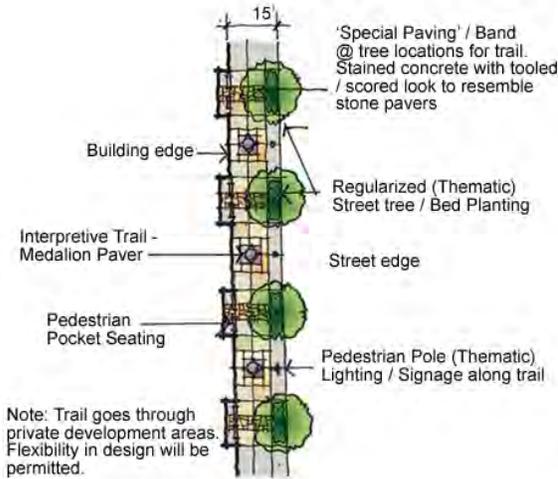


Eastern Urban Center
OTAY RANCH

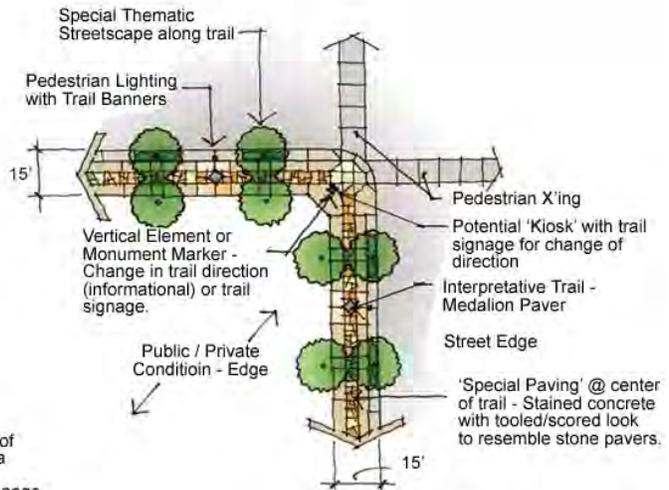


Exhibit III-33

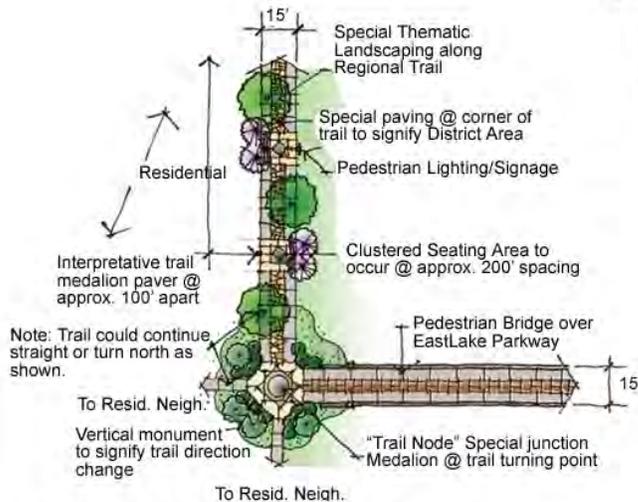
Urban Landscape Character Regional Trail



Segment through Private Development



Segment Contiguous to Street



Public to Private Transition

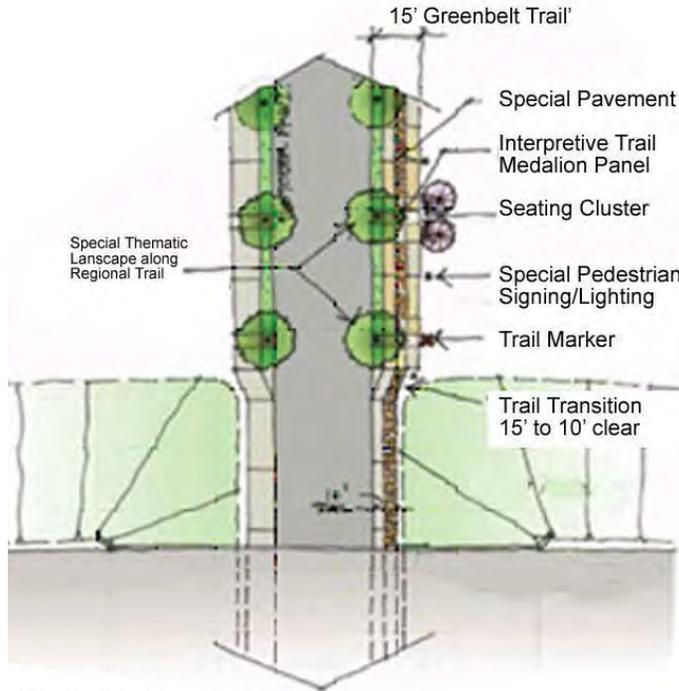


**Eastern Urban Center
OTAY RANCH**

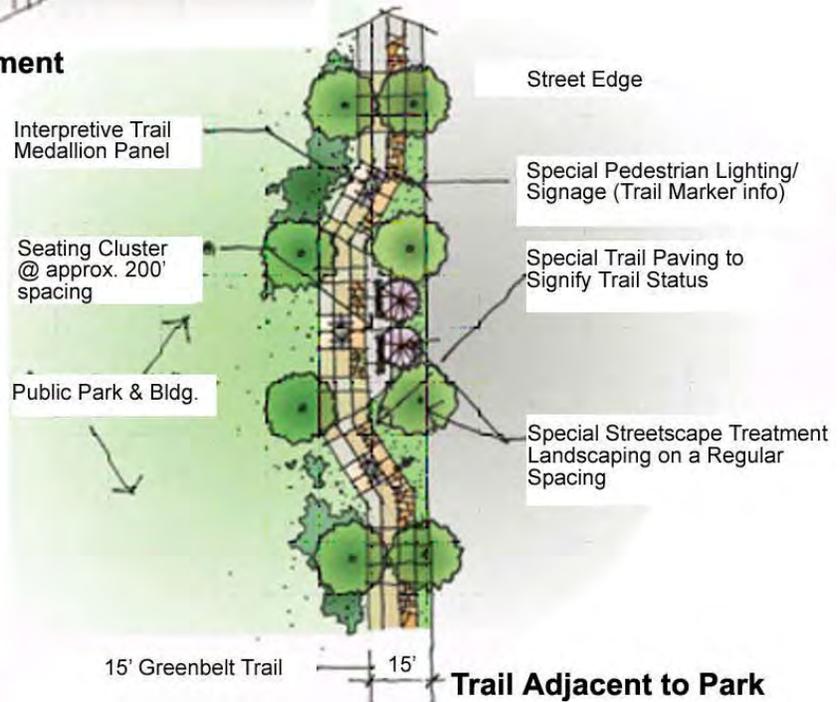


Exhibit III-34a

Urban Landscape Character Regional Trail



Bob Pletcher Way Segment



Trail Adjacent to Park



**Eastern Urban Center
OTAY RANCH**



Exhibit 34b

Pedestrian Corridors

20' Wide Section

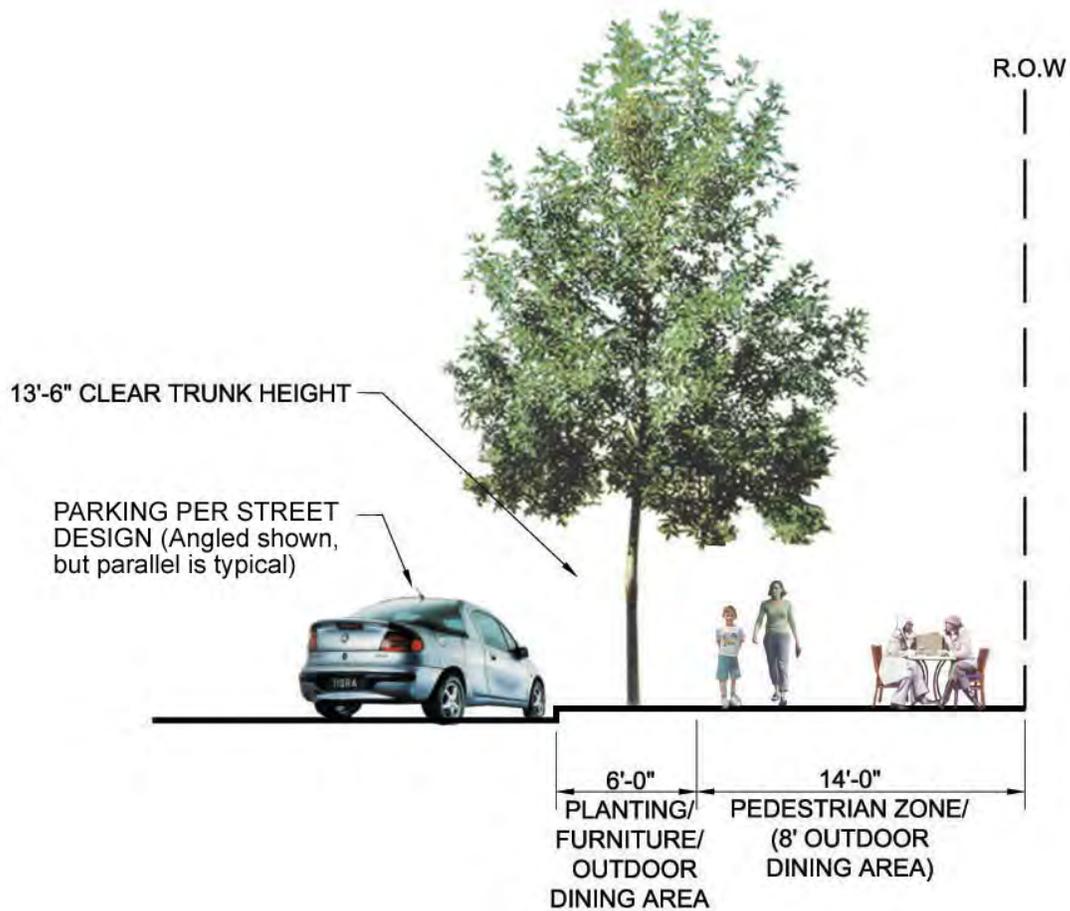
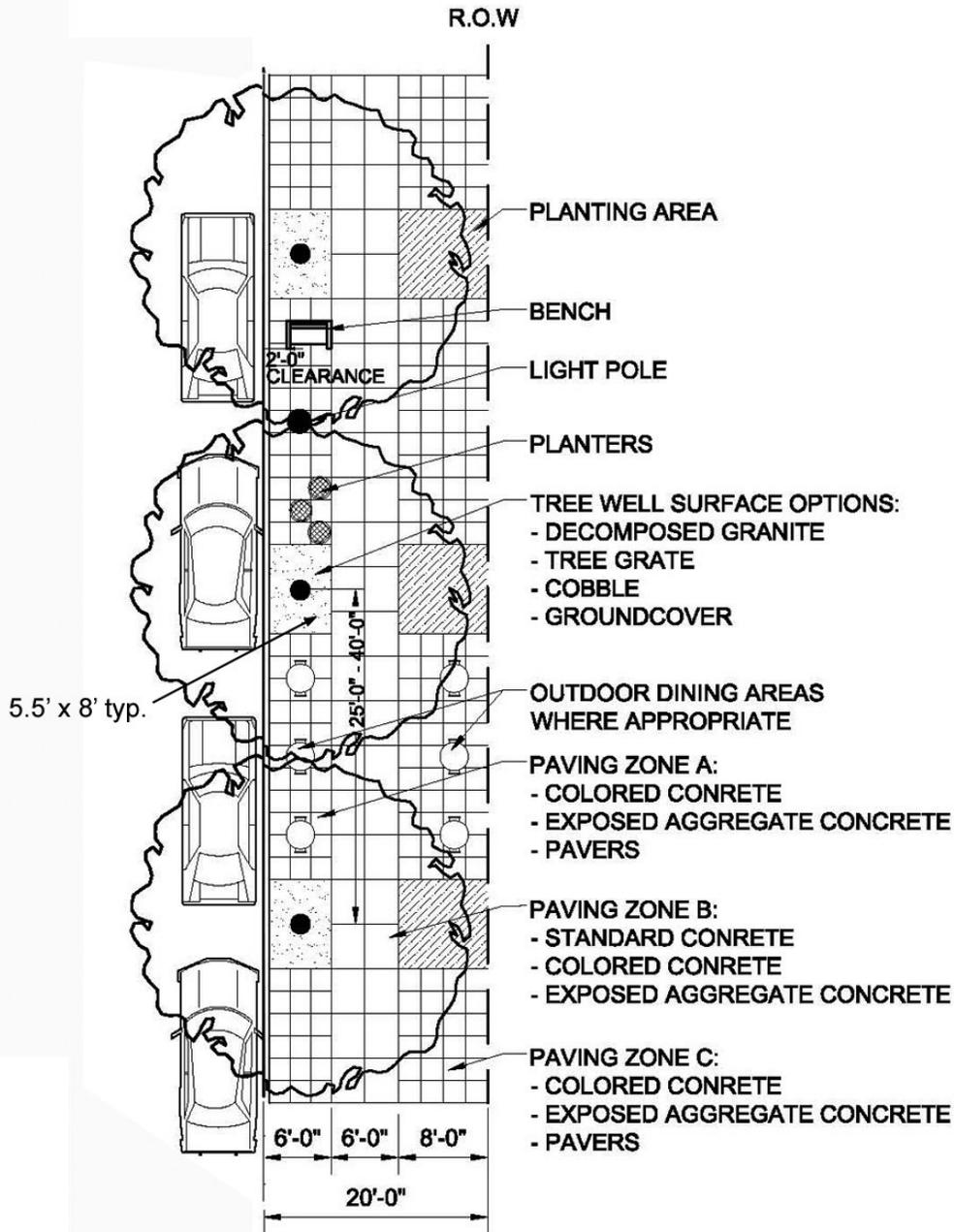


Exhibit III-35

Pedestrian Corridor 20' Wide Plan View

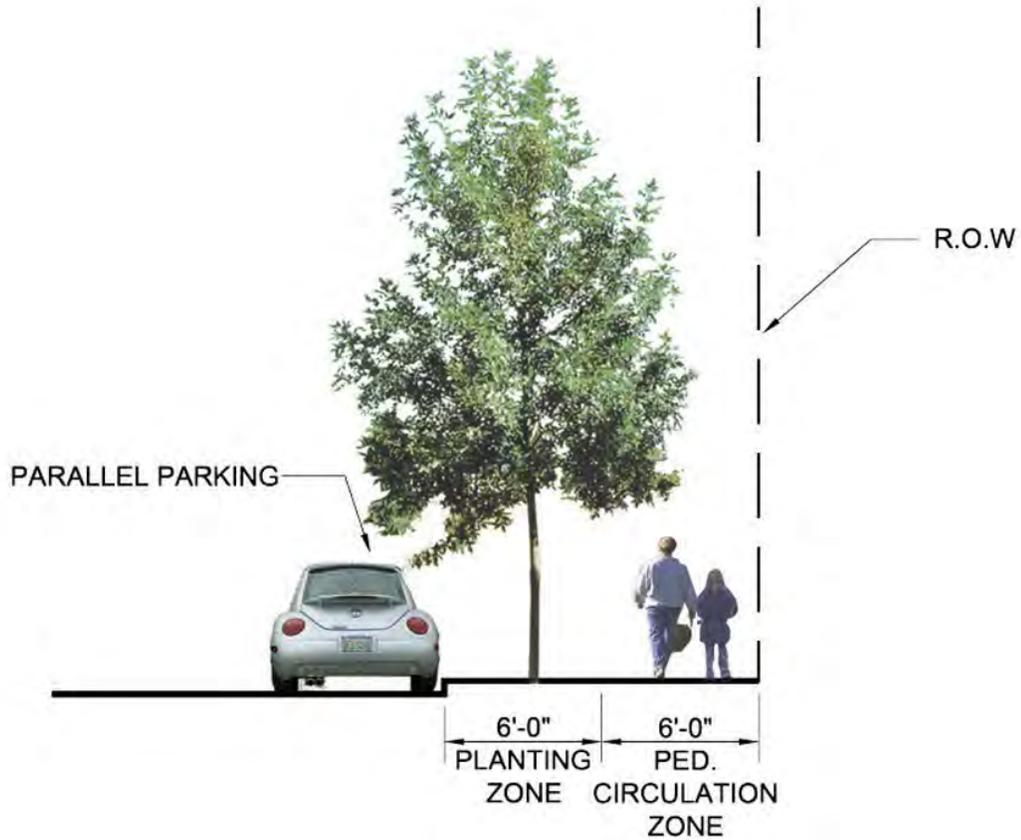


**Eastern Urban Center
OTAY RANCH**



Exhibit III-36

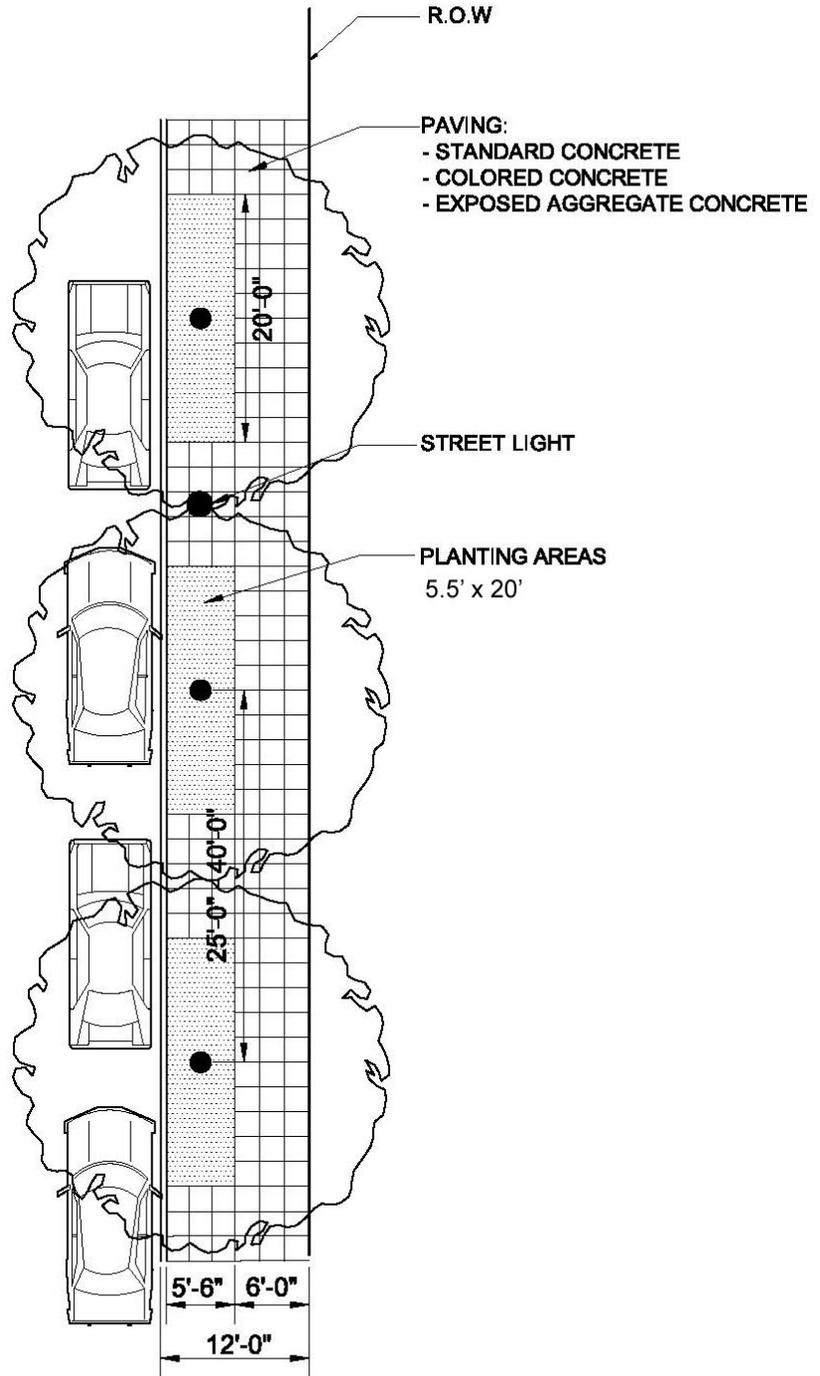
Pedestrian Corridor 12' Wide - Section



**Eastern Urban Center
OTAY RANCH**



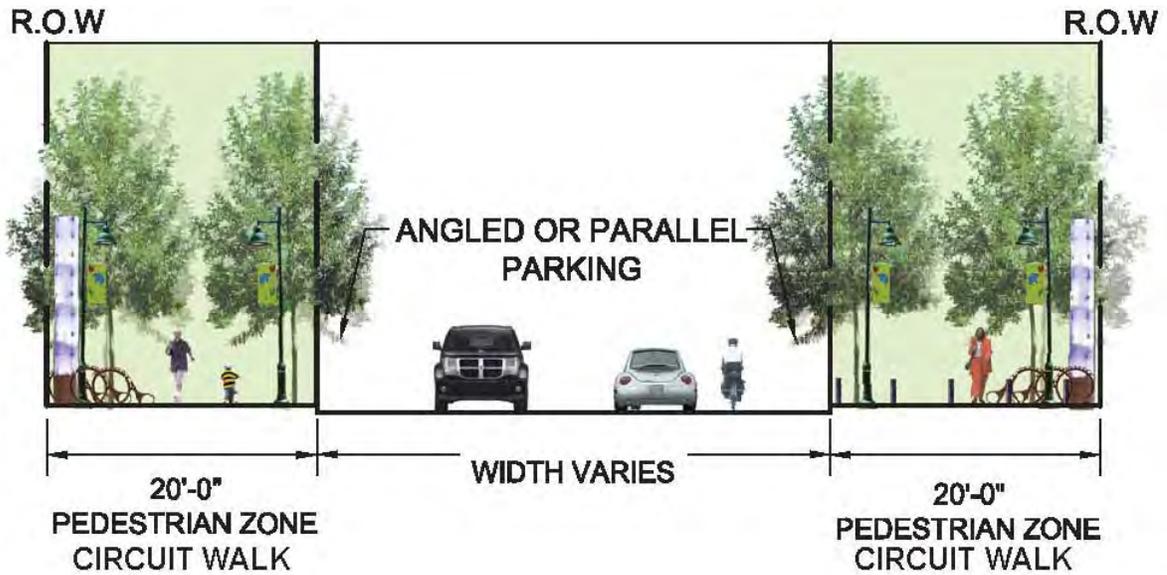
Exhibit III-37



E.U.C. 12'-0" WIDE PEDESTRIAN CORRIDOR - PLAN

Exhibit III-38

Pedestrian Corridor Circuit Walk



Sandblasting can be used to decorate and label the streetscape.



Decorative paving options

Exhibit III-39

Pedestrian Corridor Jogging Path

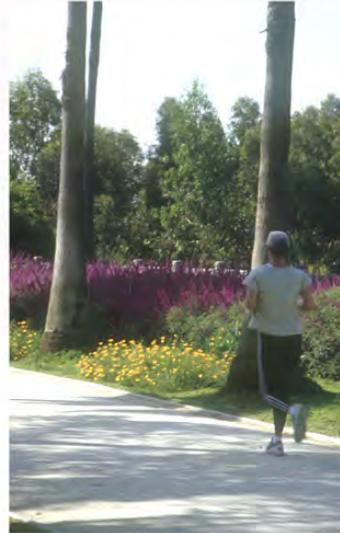
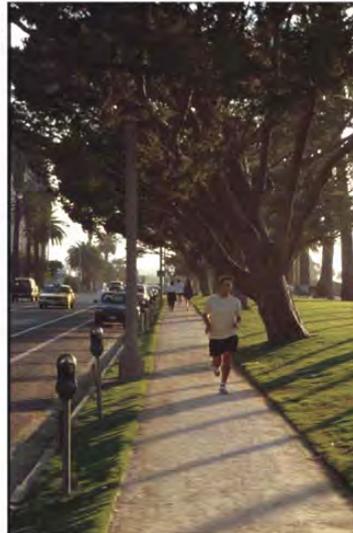


Photo precedents reflect appropriate surface materials and setting



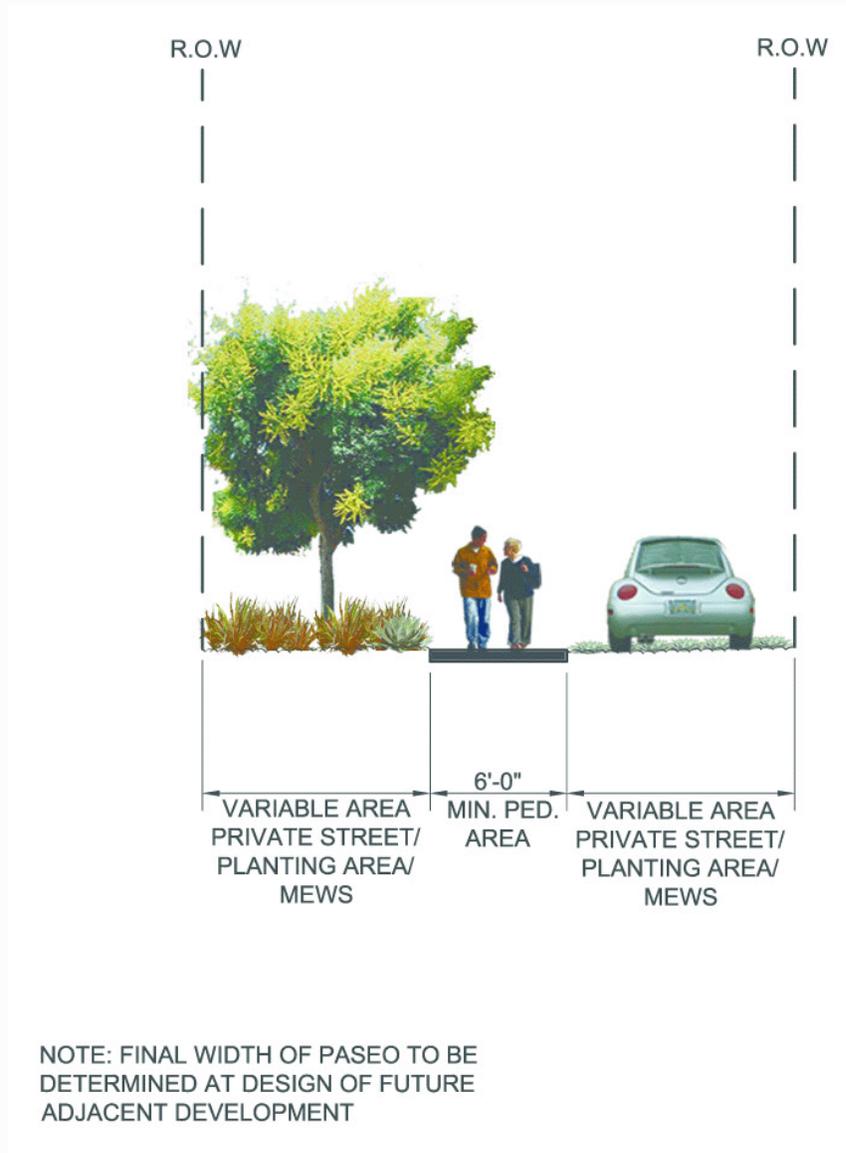
Eastern Urban Center OTAY RANCH



Exhibit III-40

Pedestrian Corridor

Paseos - Section View



Eastern Urban Center

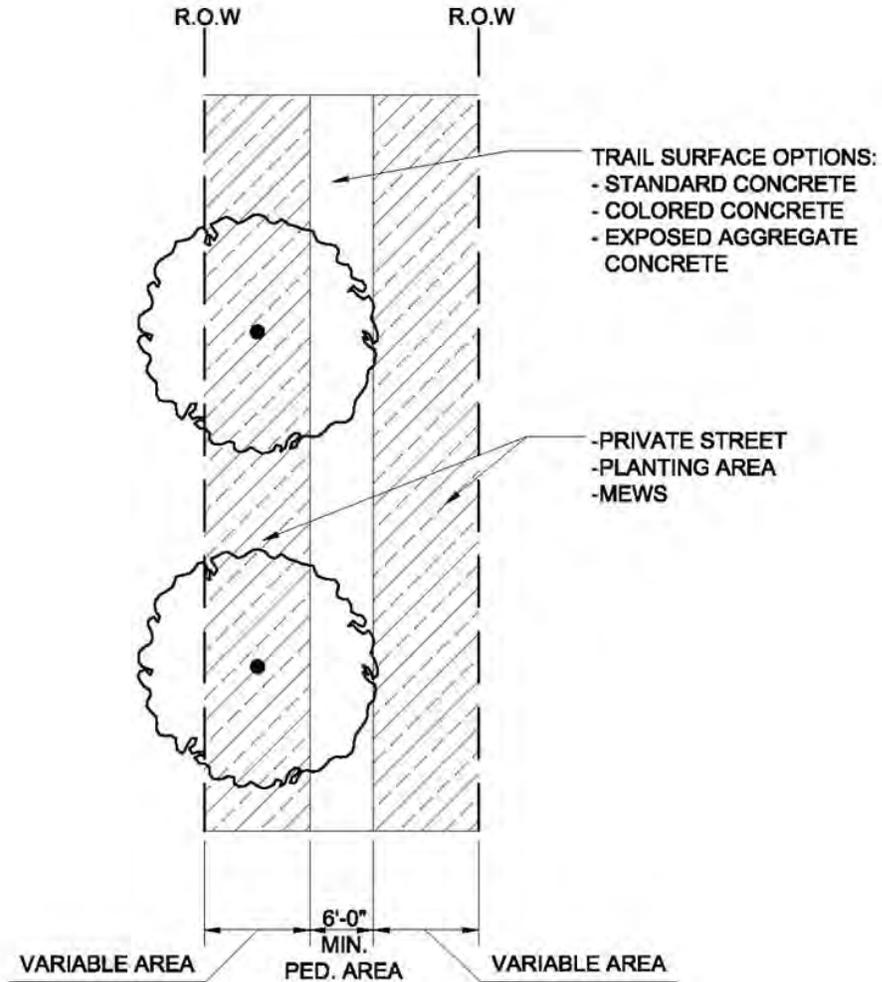
OTAY RANCH



Exhibit III-41

Pedestrian Corridor

Paseos - Plan View



NOTE: FINAL WIDTH OF PASEO TO BE DETERMINED AT DESIGN OF FUTURE ADJACENT DEVELOPMENT



Eastern Urban Center
OTAY RANCH



Exhibit III-42

03.09.000 Overall Design Character and Urban Form

03.09.001 Character Description

- a. Intent: The Eastern Urban Center is intended to have a quality, high density/intensity, mixed-use urban form. The form includes multistory buildings of various heights with specific relationships to the pedestrian-oriented streetscapes detailed in Chapter III. The design requirements for the individual buildings are less specific, requiring high quality and functional design. The overall building height and massing reflects appropriate transitions to surrounding areas as required in the District Requirements in Chapter II, herein. The overall effect of the buildings collectively is important because it is an essential component to the desired character of the *EUC*. This collective design character of buildings within the *EUC* is the urban form of the *EUC*. Together, the urban form and streetscapes define the design character of the *EUC* as experienced from the street. The urban form requirements of this chapter shall be combined with the streetscape requirements. Additionally, Chapter II includes design standards and requirements for each district.

Recognize that all sides of the building façade are elevations that require design consideration to enhance the adjacent pedestrian environment. Look to maximize the pedestrian character on all street-facing facades with vision glass where appropriate; or measures such as light spill, visual interest, or appropriate screening where the building function does not support windows.

- b. Exceptions: The illustrations portrayed in this section offer a "three-dimensional vision" for the *EUC*. As such they depict intent rather than specific requirements. Textual guidelines also reflect intent and alternative solutions may be considered provided they adhere to the overall vision. In summary, variations may be accommodated, provided that development proposals conform to the vision described throughout this and other applicable sections of the SPA Plan.



Exhibit III-43

c.

Design Principles

i. Public Spaces

- (1) An interconnected network of public plazas, paseos, and open green spaces
- (2) Buildings placed close to street framing outdoor public spaces
- (3) Town squares / plazas as outdoor rooms
- (4) Enhanced pedestrian environment with canopy shade trees
- (5) Events: festivals, farmers' markets, concerts

ii. Mixed-Use

- (1) Walkable connected neighborhoods
- (2) A mix of residential, commercial, and civic uses both vertical and horizontal
- (3) Flexible community resources
- (4) Shops and restaurants located within walking distance of offices and residences
- (5) Open space network integral to social life of the community

iii. Neighborhood Livability

- (1) Residential enclaves have a discernible center
- (2) Diverse housing types (townhomes, apartments, mid-rise)
- (3) Distinctive neighborhoods with individual character
- (4) Shops and offices within walking distance of housing
- (5) Hierarchy of public, semi-public, and private outdoor spaces
- (6) Pedestrian Friendly
- (7) Sidewalk activity / active ground floor uses
- (8) Primary residential entrances along sidewalks
- (9) Walkable scales
- (10) Canopy shade trees and outdoor amenities enhance pedestrian environment

d. Relation to Public Realm

Public realm elements including parks, paseos, plazas, trails, streets, and streetscapes all converge forming a connective network that provides for a walkable community with pedestrian priority and creates a sense of place. Park and plazas spaces dispersed throughout the EUC act as buffers, quiet retreats, children's play area, and social gathering spots. Main Street sets the standard for this mixed-use community with ample street trees, pedestrian scaled lighting, connection to plazas and trails, and buildings that frame public space, all of which are things that are carried on throughout the site at various scales. A distinctive identity for the EUC is formed through these numerous public realm elements generating this visually pleasing place to live, work, and shop.

Illustrative Plan



* Note: This exhibit is an artist's conception of the ultimate build out of the EUC. It represents only one of the various alternatives that would be consistent with the intended vision for the EUC.

Exhibit III-44

Aerial Perspective Looking North



Exhibit III-45a

Aerial Perspective - Main Street



* Note: This exhibit is an artist's conception of the ultimate build out of the EUC. It represents only one of the various alternatives that would be consistent with the intended vision for the EUC.

Exhibit III-45b

Urban Form District Map

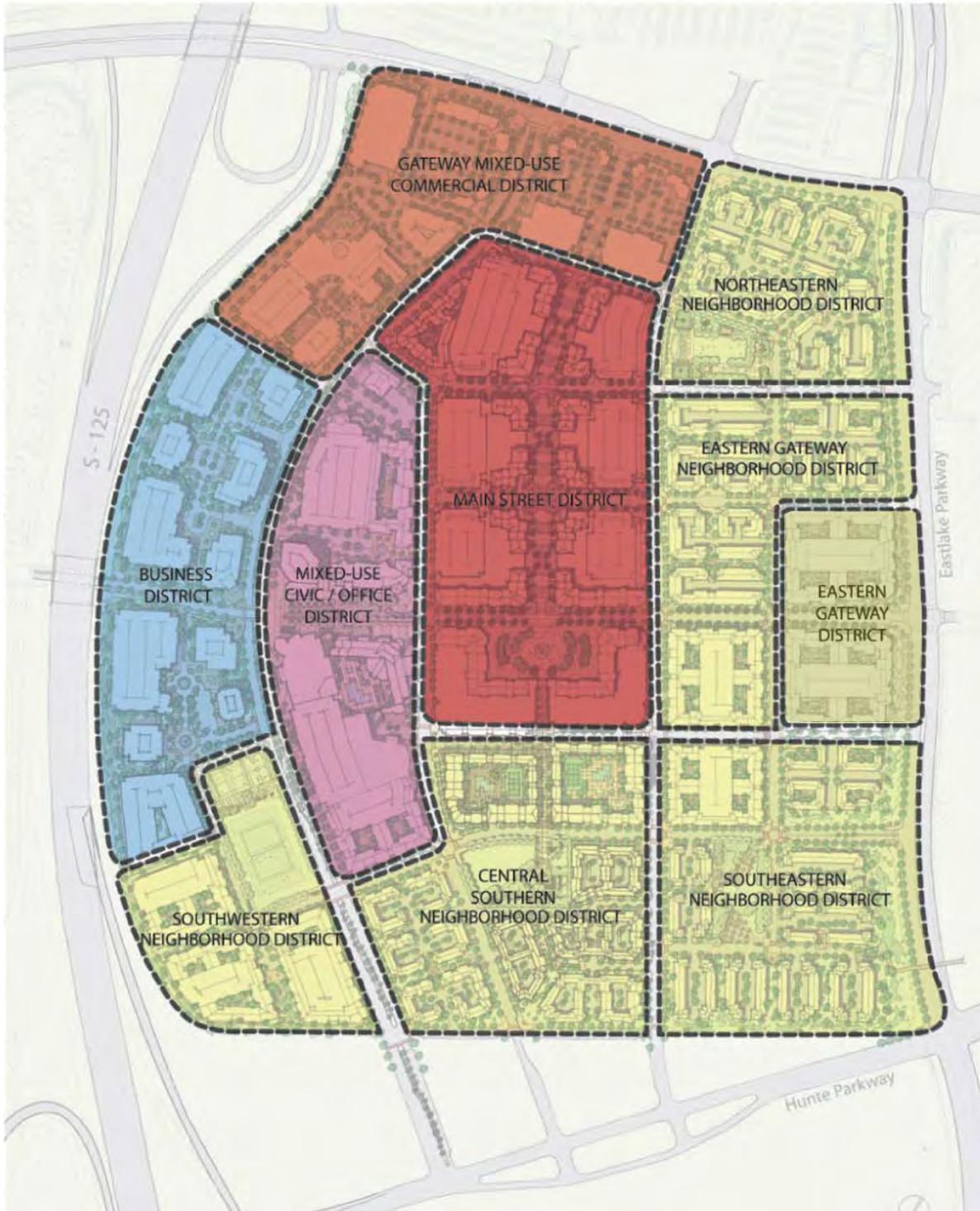


Exhibit III-46

e. Utilities: All utility connections shall be designed to coordinate with the architectural elements of the site so as not to be exposed to public view. Transformers and/or meter box locations shall be included in the site plan with an appropriate screening treatment. Some examples of utility screening are shown on Exhibit III-47. Where the utility device does not have to be located in view of pedestrians along the street, they can be hidden behind low walls and landscape elements. Initial device installations for backbone utilities, prior to adjacent buildings being constructed, do not have to be screened.

Utility Screening Options



Glass block Option



Building Option



Concrete Option



Exhibit III-47

03.09.002 Average Building Height & Map

- a. **Measuring Building Heights:** Building heights shall be measured in accordance with CVMC 19.04.038. Exemptions to height limits shall be in accordance with CVMC 19.16.040.
- b. **Building Height Averaging Guidelines**
 - i. **Height Averaging Provision:** The following Exhibits III-48 and III-49(Average Height Districts Map), allows for a process of "height averaging" within each of the outlined districts, whereby a building within any of the outlined areas may be higher or lower than the established height number for that particular area.
 - ii. Higher and lower buildings counterbalance one another within the same height district (each district is outlined by a dark grey line on the Building Height District Map) to achieve a project that protects view corridors and architecturally significant buildings, structures, or landscapes. A pleasant visual mix and transition in height and massing is achieved through height averaging.
 - iii. For the purpose of determining building height for the "height averaging provision", where a building footprint features multiple heights, the dominant building mass shall provide the height measurement for height averaging, thereby excluding minor setbacks and minor height projections from the calculations. The block build out plans shall include one or more blocks as required to demonstrate compliance with the minimum average building standard.
 - iv. One *story* buildings have a 25' minimum unless otherwise noted.
 - v. The following diagram shows an example of height averaging where in the outlined district shown below (with a relative building height of 5 stories) heights range from 3-6 stories, allowing for a level of flexibility.

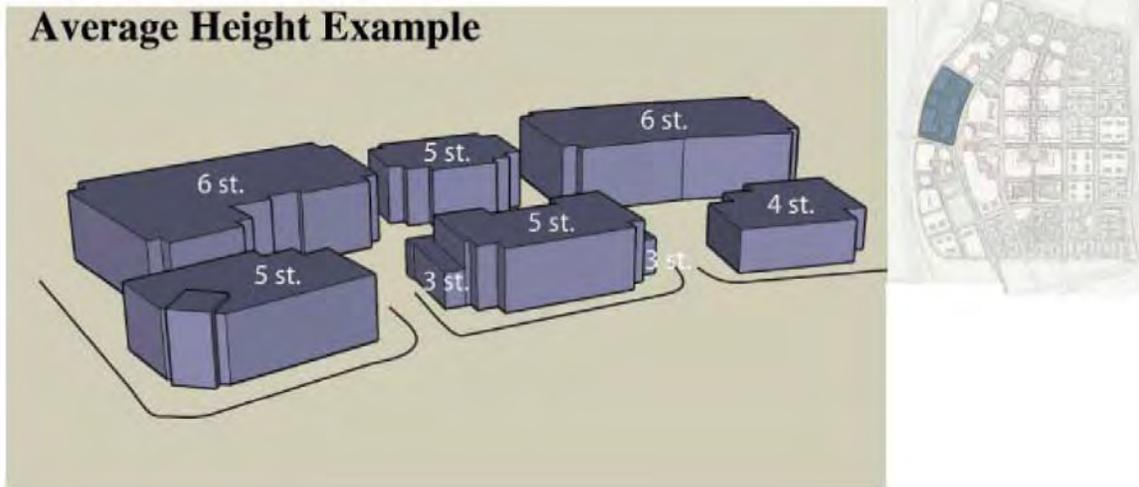


Exhibit III-48

- vi. Exceptions: Exceptions will be considered that are not factored into height averaging, specifically buildings that represent *iconic* architecture, including towers, that punctuate important intersections, prominent locations, or similar conditions. Similarly, specific uses requiring single *story* buildings (such as high end restaurants, clubhouses, and civic buildings). All buildings on lots 11, 12, 20, and 21 may be 2-story in height, except as otherwise specified below, and exempt from height averaging. For these lots, the following criteria apply:
- (1) that buildings fronting “D” Street are minimum 3-story and wrap at the corners of “F” Street, “H” Street, & “K” Street;
 - (2) that buildings fronting on EastLake Parkway, “H” and “K” Streets include enhanced architectural treatment and punctuate the top building line with vertical elements to avoid a continuous 2-story roof line. Such treatments can include, but are not limited to, incorporation of taller elements, use of materials, colors, massing, articulation, etc. to create a unique appearance.
 - (3) that the following intersections have either 3-story buildings or 2-story buildings with enhanced vertical features to emphasize height at intersections.
 - (a) Southwest corner of intersection of EastLake Parkway and “F” Street (at this location, 3-story buildings are encouraged, but not required);
 - (b) Northwest and southwest corners of the intersection of EastLake Parkway and “H” Street; and,
 - (c) Northwest corner of the intersection of EastLake Parkway and “K” Street.

- (d) Along the “F” Street frontage of lot 11, to enhance the urban appearance and to provide a transition to the 3-story buildings of the “D” Street frontage;
- (4) All 2-story buildings fronting internal streets and main pedestrian corridors, such as along the BRT lane, shall feature high quality urban architecture in accordance with the applicable District Design Guidelines and Design Regulations, Residential, Building Typologies and Section 03.10.000 - Architectural Design Guidelines of the EUC Form Based Code.
- (5) All building heights shall be determined in accordance with Section 03.09.002 of this Form Based Code.
- (6) Provision for height district blending: As shown on Exhibit I-4, lot numbers 17 and 24 are west of the Regional Trail, south of Metro Park, and lot numbers 18 and 23, are east of the Regional Trail. Lots 17 and 24 are designated 3-story and 4-story respectively. Lots 18 and 23 are also designated 3-story and 4-story respectively. The 3-story and 4-story height districts for lots 17 and 24 may be blended as a mix of 3 and 4 story buildings, all 3-story, or all 4-story, providing they are approved as a comprehensive plan for both lots. This height determination shall be as approved during Design Review and shall be based on the overall design meeting the intended urban character rather than height as the sole design determinant. This height district flexibility is also permitted for lots 18 and 23. Notwithstanding this provision for height blending, covered decks on 3-story buildings will not be counted as a story for any of these lots. Additionally, Lots 23 and 24 shall be limited to a minimum building heights of 3-stories.

Building Height Determinants

Variations & Design Considerations

Stepped down building heights creates form and interest to overall building.

Varying heights and vertical color changes emphasizes height and adds interest



Four stories with varying heights add interest. Where a building features multiple heights, the dominant building mass (>50%) shall provide the height measurement for height averaging.

A story is the useable area between the floor and the ceiling in a structure



Note: This exhibit is an artist's conception that only represents one of many design solutions that would be consistent with the intended vision for Millenia. Refer to Section 03.09.002 b vi (6) for certain exceptions on covered roof decks.

Exhibit III-48a

Building Height & Stories

Regulatory References



5-Story Chelsea Product

4-Story Product

Building Height

FBC Exhibit III-49 establishes Minimum Building Height. It must meet either the minimum height feet or stories.



Building Stories

Note: This exhibit is an artist's conception that only represents one of many design solutions that would be consistent with the intended vision for Millenia. Refer to Section 03.09.002 b vi (6) for certain exceptions on covered roof decks.

Exhibit III-48b

Building Heights 4-5 Story Transitions



Front Elevation - 4 Story "Millena 6-Plac" Project

Front Elevation - 5 Story "Chelsea" Project



Note: This exhibit is an artist's conception of the development of Millena, District 6. It represents only one of various alternatives that would be consistent with the intended vision for Millena.

Exhibit III-48c

Regulating Plan Minimum Average Building Heights



Minimum Average Building Height Districts*

Stories	Height in Feet	
	Residential	Non-residential
1	25	25
2	30	35
3	35	40
4	45	50
5	55	70

* Must meet either stories or building height in feet, except that 1-story buildings must be a minimum of 25' in height.

Other Height Standards

- Lot ID Number (refer also to Section 03.09.002 b vi 6)
- Locations where taller buildings are appropriate.
- 3-Story minimum height fronting adjacent streets.
- Buildings on these lots may be higher than those on Main Street.
- 1 Story Retail permitted fronting the park (Refer to Note 6).
- Potential School location, which does not factor into average building height calculations.
- 3-Story buildings or buildings with enhanced vertical features to emphasize height at intersections.
- Provide enhanced architectural treatment & punctuate the top building line with vertical elements to avoid a continuous 2 story roof line (Refer also to Chapter 03.09.02b vi)

Notes:

1. Iconic Architecture: building architectural design representational of its special location or prominent address; special architectural features; treatment and materials, to create a distinctive architectural facade and feature at prominent address or street corner.
2. This conceptual plan illustrates the minimum average height for each district indicated at full build-out. It does not establish the minimum or maximum heights. Individual buildings may deviate from the minimum average height so long as the overall average within the district is maintained.
3. The average heights include parking within the building, but excludes separate parking structures or accessory structures. Refer to the text in this chapter for additional explanation and methods of calculating height.
4. Refer also to chapter 02 for the minimum and maximum building height standards for each street frontage.
5. Refer also to Exceptions in Chapter 03.09.02b vi, herein.
6. Retail fronting Park P-3 need not satisfy the building height in feet and shall be excluded from the building height averaging calculations.
7. Refer also to the Tables of Requirements for more height regulations.



Exhibit III-49



03.10.000 Architectural Design Guidelines

a. Architectural Design Principles

i. Human-scaled Design

- (1) Sense of place and community identity
- (2) Buildings relate best to one another when utilizing base, middle, and top forms as their primary organizational method.
- (3) Windows, balconies, loggias, and parapets broken down to a scale appropriate to the human form
- (4) Rich in eye-level detail
- (5) Recognize that all sides of the building facade, that are exposed to the public, require design consideration to enhance the pedestrian experience. Building designs shall maximize the pedestrian character on all street facing facades with vision glass, where appropriate; or measures, such as: architectural treatments; light spill; or, appropriate screening where the building function does not support windows.



ii. Building Massing and Orientation

- (1) Clustering buildings together creates more shade-casting mass
- (2) Deep set windows reduce heat gain and increase shade without external shade structures
- (3) Buildings should be attractive from all vantage points, but their primary facades should be oriented toward the public street to provide form and function to the streetscape
- (4) Consider the front facade of a building as the primary contributor to maintaining pedestrian interest and activity along parks, plazas and streets



iii. Ecological Design

- (1) Regional building traditions / climatic response
- (2) Green building practices
- (3) Energy efficient (e.g. thru-ventilation, such shading)



Exhibit III-50

iv. Sustainable Development

- (1) Complementary mix of uses (e.g. residential, commercial, civic, open space)
- (2) Compact development pattern
- (3) Transit and pedestrian oriented development

v. Materials and Colors

- (1) Use elements that communicate permanence and quality, especially at street level and facades that face major public spaces
- (2) Utilize materials and colors that are complementary to the overall design and that enhance a building's visual quality
- (3) Treat all sides of a building with compatible materials and colors.
- (4) Rely on accent materials and colors to articulate the facade

b. High rise architecture and compatibility

- i. Ensure high rise buildings display a high level of quality to create signature architectural statements.
- ii. Recognize the need for appropriate transitions in building form for high rise buildings.

**Building
Type
Precedent
Diagram**



legend:

1. Active ground floor uses
2. Transparent store fronts
3. Integrated signage
4. Residential or office above
5. Use of awnings, hanging signage, & other elements to improve the pedestrian environment
6. Projected balconies when appropriate
7. Special treatments at entry vestibules
8. Articulated base, middle, & top
9. Step backs at top floor terraces

Exhibit III-51

03.11.000 Building Placement and Orientation Guidelines

03.11.001 Placement

- a. Locate and mass buildings to consistently frame and define public space. An especially strong relationship between the building and street is encouraged. Exceptions may be considered in “select transitional” areas for example along major arterials.
- b. Reinforce plaza edges. Building placement shall delineate the park promenade and promote park views.
- c. Orient buildings so as to address the street and/or a park. Buildings should become an integral part of the street fabric and contribute to a human-scaled environment.
- d. Build at or near the sidewalk edge so as to place strong visual emphasis on the street. Exceptions may be considered in “select transitional” areas, for example, along major arterials.
- e. Allow minor modulations in the street setback (to accommodate entry forecourts, recessed shop entries / vestibules, or ‘eroded’ building corners, etc) provided continuity of the street wall is maintained.
- f. Parking garages that are adjacent to the street right-of-way of the EUC’s primary grid of public streets shall not be located on opposite sides of the street right-of-way for a distance of more than 200 feet or 50% of the length of the block, whichever is less. The EUC primary grid of public streets are “A”, “B”, “C”, “F” and “K” Streets.
- g. There are slopes along EastLake Parkway and the entries that require a deeper setback. Adding retaining walls, or combinations of slopes and walls, in these slope areas is permitted to bring the buildings closer to the street.

03.11.002 Orientation

- a. Provide for the functional and visual integration of buildings, vehicular access and parking, services, on-site pedestrian circulation, and ‘outdoor rooms’ such as plazas and courtyards.
- b. Encourage developments that are focused on and activate the street environment.
- c. Provide human-scaled blocks that allow convenient passage through districts.

Regulating Plan Building Setbacks



-  0' setback
-  0'-5' setback
-  0'-10' setback
-  0'-15'+ setback

Note: Setbacks shall be measured from the street right-of-way or property line. Refer to text for additional description of setback standards.



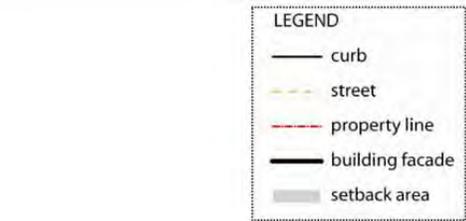
**Eastern Urban Center
OTAY RANCH**



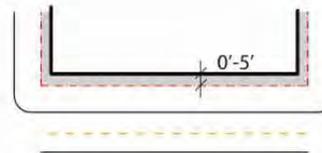
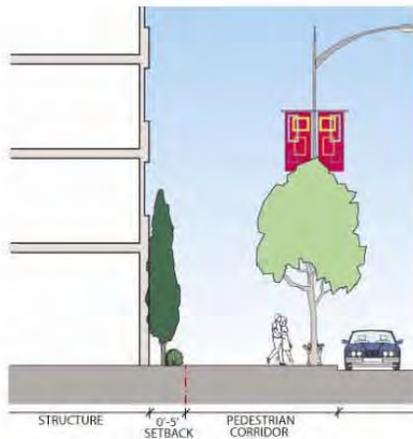
Exhibit III-52

03.11.003 Building Setbacks Defined

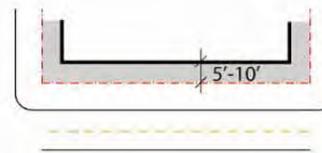
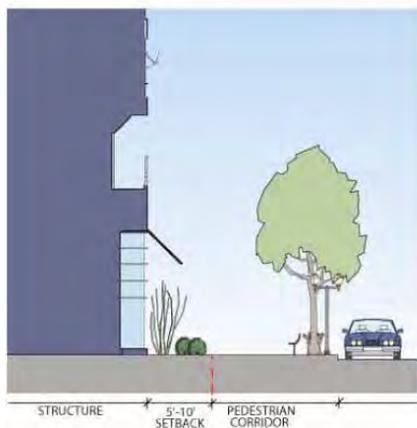
a. Measuring setbacks



Zero 0' Setback



0'-5' Setback



5'-10' Setback

Exhibit III-53

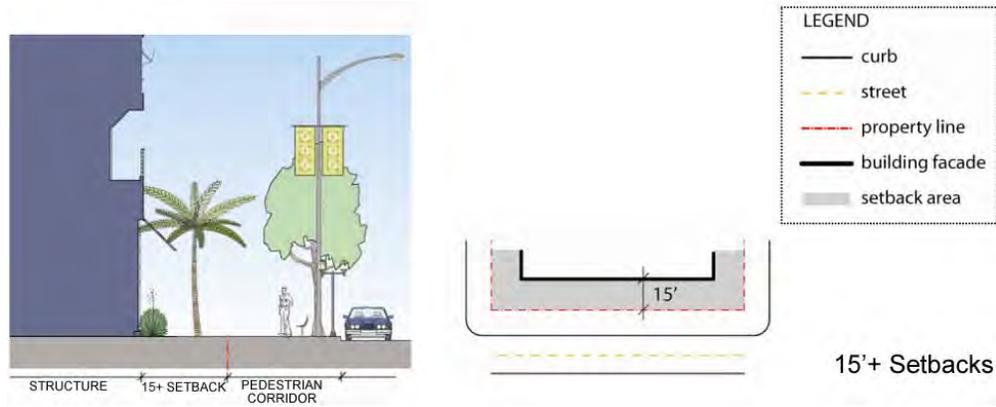


Exhibit III-54

b.Exceptions: The Building Setbacks Map, Exhibit III-52, conceptually presents average building setbacks at full build-out. Exceptions or deviations from this diagram may be accommodated through the design review process. The following conditions will be considered for an exception:

- i. Building recessions or projections to accommodate iconic architectural elements consistent with locations specified in the urban design diagrams.
- ii. Building forecourts, recessed building and shop entries, or recessions to accommodate outdoor dining (especially at locations identified in the urban design diagrams) provided that these recessions do not extend across a majority of the building facade, and no more than 30% of the building facade where the urban design diagram specifies an active ground floor within the Main Street District.
- iii. Other minor facade modulations that create building interest providing the projection or recession is not in excess of three (3') feet. This is not intended to limit canopies, awnings, balconies, or similar features.
- iv. Minor deviations to accommodate a straight wall or a curving roadway, provided there is a consistent building edge that approximates the setback line to the extent feasible, while allowing for other exceptions as noted herein.
- v. Deviations to highlight or differentiate a civic or landmark / iconic building, provided that this does not amount to more than one exception per block face, typically associated with a public plaza.
- vi. Non-typical setbacks conditions described under Building Placement Typologies, including:
 - (1) building setbacks to accommodate a public plaza;
 - (2) commercial setbacks along arterials and perimeter four-lane roads that require auto-oriented visibility and access, allowing parking lots with a landscape buffer fronting the street'
 - (3) breaks in the building wall to accommodate occasional mid-block paseos and passages.
- vii. Setback deviations as necessary to accommodate slopes or grade differences along the street or building setback line, and unique operational characteristics (queuing, access or other factors) which preclude them from meeting setbacks.

03.11.004 Building Placement Typologies

a. Typical Conditions

i. 0' Setback

- (1) Mixed use bldg. Directly fronting street
- (2) Entries directly front sidewalks



Exhibit III-55a

ii. 0' - 5' Setback



Exhibit III-55b

iii. 5' - 10' Setback

- (1) Residential stoops create buffer from sidewalk, noise, etc.
- (2) Small landscaped setbacks



Exhibit III-55c

iv. 15' + Setback

- (1) Residential front yards incorporating shallow street oriented stoops and patios
- (2) Landscaped setback / slope embankment / adjacent trail



Exhibit III-55d

b. Non-typical Conditions

i. Active plaza fronting street



Key
 1-special paving on fronting street
 2-active uses surrounding plaza
 3-simple lighting for pedestrian safety and comfort
 4-decorative planters adjacent curb
 5-prime location for pedestrian directionals
 6-retail kiosk adds vitality
 7-buildings define public space

Exhibit III-56a

- (1) Active plazas are a key element of urban place-making.
- (2) Public urban plazas are most successful when they offer direct access to the sidewalk / street (differing from more private courtyards in this respect).
- (3) Urban plazas need to be well defined by adjacent / surrounding buildings.
- (4) Active uses should be encouraged around the perimeter of these spaces.
- (5) Plaza should be comfortable for human occupation and use, enriched with lighting, site furniture, plantings and canopy trees.



Exhibit III-56b

ii. Entries do not take direct access from street (portal from street to access entries)



Key
 1-gateway element to announce paseo, and enhance street
 2-amenities including benches and planters
 3-secondary shop entrances
 4-pedestrian scaled lighting
 5-generous walkway for pedestrian ease

Exhibit III-56c

- (1) Allow for entry to be taken off of paseos and similar pedestrian passages.
- (2) Pedestrian passages and paseos are an important element of urban place-making and an important part of the pedestrian circulation network.
- (3) Pedestrian passages and paseos should display a public character and be enriched with site furnishings, outdoor lighting, and plantings.
- (4) Secondary shop entrances, primary entrances to small retail shops, and entries to residential units are appropriate along pedestrian passages and paseos.
- (5) Pedestrian passages and paseos may be defined by distinctive gateways that enliven the street.



Exhibit III-56d

iii. Commercial not fronting directly on street



Key
 1-architectural details
 2-landscaped areas break down surface parking
 3-outdoor seating

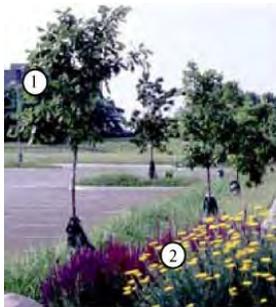
Exhibit III-57a

- (1) Limited to perimeter arterials and perimeter four lane roads (esp. Gateway Mixed-use Commercial District).
- (2) Location along arterial provides a unique opportunity for a large-scale (anchor) commercial uses that will benefit from auto visibility.
- (3) Condition incorporates visible surface parking fields and allows direct views of the buildings from the street, addressing tenant demands.
- (4) Pad retail introduced at key locations along the street enhances street scene, frame vehicular entrances and view corridors, and breaks down the extent of surface parking lots.



Exhibit III-57b

iv. Parking lot with landscape buffer / screen fronting street



Key
 1-trees provide visual relief for parking lot
 2-flowers and low shrubs as landscape buffer/ground cover

Exhibit III-57c

- (1) Limited condition found within the Gateway Mixed-use Commercial District (associated with condition of commercial not fronting directly on street)
- (2) Also a potential temporary condition occurring prior to full build-out of a block
- (3) Low landscape treatment important as visual buffer between sidewalk and parking.
- (4) Landscape treatment to include low shrubs and may incorporate low, decorative walls and trellis.
- (5) Canopy trees within parking lots also ameliorate visual impact.



Exhibit III-57d

v. Buildi

ng heights range from 1 - 4 stories on a single street

- (1) Condition appropriate to mixed-use, urban setting.

- (2) Allows introduction of unique use or building typology appropriate to surroundings.
- (3) Varied heights provide opportunity for visual relief and additional architectural interest.
- (4) Datum line (often defined by first floor cornice) operates as the key unifying element along the street.
- (5) Also relies on transitional height / massing to avoid abrupt changes in scale.



Key

- 1-architectural details emphasize height ranges
- 2-color and material differentiation at varying heights adds architectural interest
- 3-height transition with articulated parapets provide opportunities to screen rooftop equipment



Key

- 1-datum at first or second level differentiates commercial from upper level residential
- 2-height difference allows roof terrace

Exhibit III-58

03.11.005

Parking garage facades

- a. Facade treatment
 - i. Parking garage facades that are adjacent to the street right-of-way shall include one or more of the following: retail at corners, green screens, art walls, and landscape buffers.
 - ii. Parking garage facade treatments are most important in the highly active pedestrian core.
- iii. Encourage using two (2) or more of these parking structure treatments to augment the appeal of these structures as shown in exhibits A & B, while integrating these design concepts with opportunities to provide cost-

effective and aesthetically appealing natural ventilation.

Exhibit A

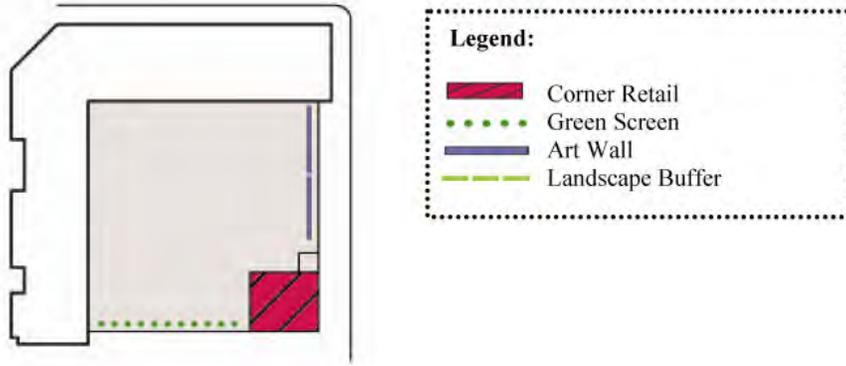


Exhibit B

parking garage elevation

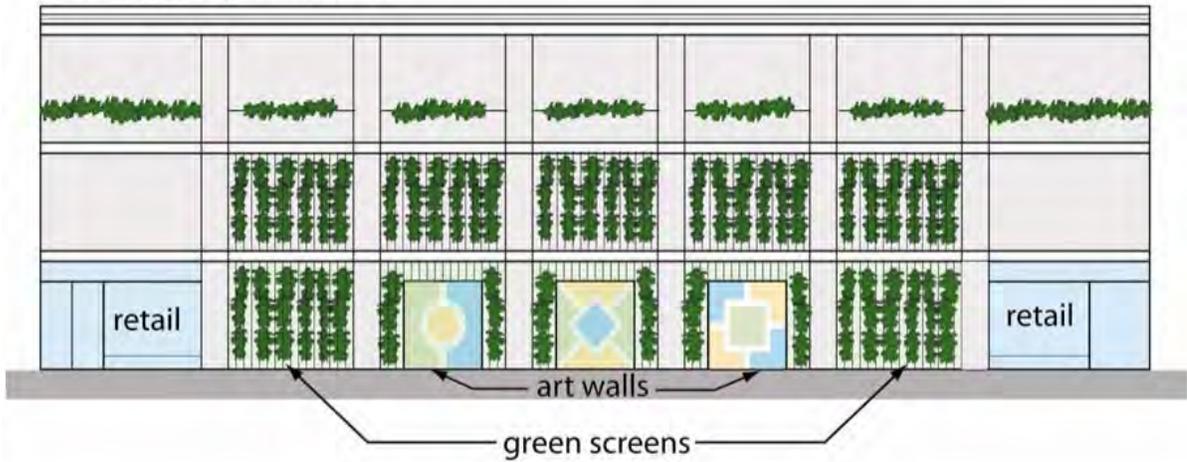


Exhibit III-59

b. Alternative 1: Retail Corners

- i. Heighten pedestrian activity through active retail uses at corners of parking structures.
- ii. Small retail components aid in avoiding expansive blank walls by adding architectural interest to the corners of parking structures.
- iii. Limit retail to key corners adjacent to primary retail areas -retail should wrap around key corners, creating dual points of interest. This may have limited application and should be incorporated where retail activity is supported.



Plan

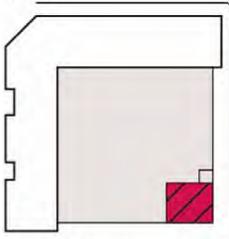
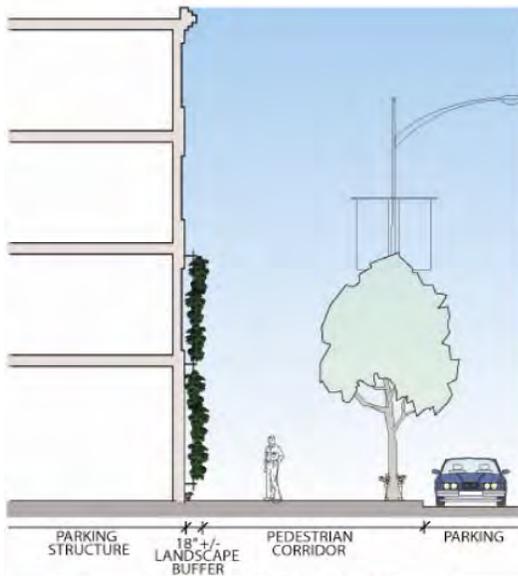
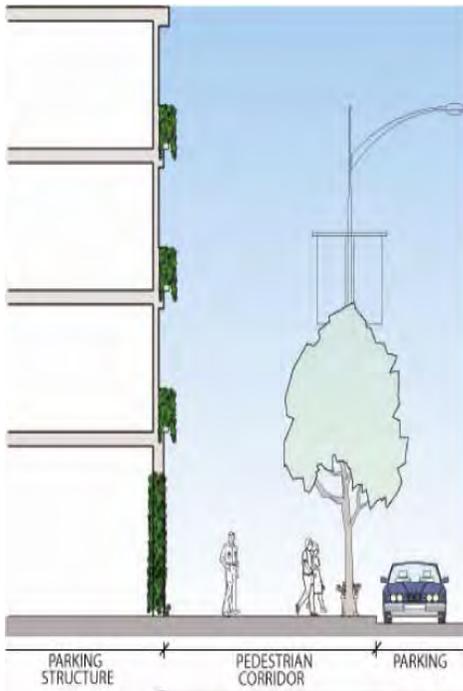


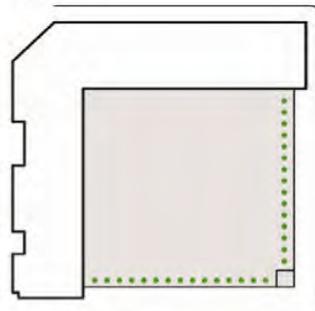
Exhibit III-60

c. Alternative 2: Green Screens

- i. Break up massing through the use of greenery on parking structures.
- ii. Green screens provide a cost effective and attractive alternative where wrapped parking is not feasible.
- iii. Unsightly parking is not only hidden from the eyes, but cars are also protected and shaded from the sun.
- iv. Implement CPTED principles in the application of green screens to allow active surveillance from the street.



Plan



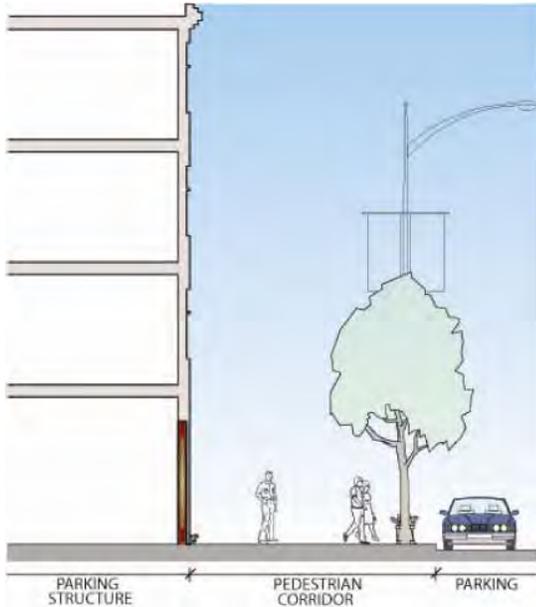
Legend:

-  Corner Retail
-  Green Screen
-  Art Wall
-  Landscape Buffer



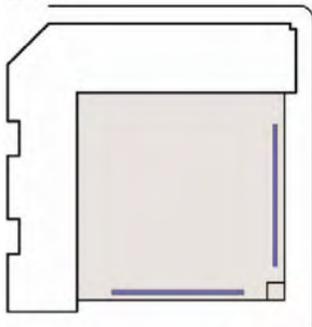
Exhibit III-61

d. Alternative 3: Art Walls



- i. Opportunities for civic artwork are created through the use of exterior art walls.
- ii. Art walls can also function as informative community signage that may change on a regular basis.
- iii. Art walls are an inexpensive alternative where fully wrapped parking structures are not feasible.
- iv. Artistic trellis features or landscaping can also be incorporated into an art wall.
- v. Art walls promote community pride and an opportunity for district identity.

Plan



Legend:

- █ Corner Retail
- ⋯ Green Screen
- Art Wall
- - - Landscape Buffer



Exhibit III-62

03.12.000 Landscape Standards and Guidelines

03.12.001 Scope:

Landscape in the *EUC* is very important in creating the public realm. Because of this landscape requirements are provided throughout the *EUC FBC*, SPA Plan and related documents. This section of the document provides requirements for:

- a. Street trees
- b. Plant materials
- c. Lighting
- d. Street furniture

Other sections provide additional requirements for landscape design, including:

- a. Pedestrian Corridors and Trails (Section 08, herein)
- b. Landscape Master Plan (Section 04, herein)
- c. Signing (Section 03.12, herein)
- d. Parks, Plazas, & Open Space (Urban Parks, Recreation, Open Space & Trails Plan)

03.12.002 Street Tree Concept Plan

- a. Street trees are critical in urban areas, as they help define outdoor spaces, provide a green counterpoint to building masses, and offer a pleasing outdoor environment for pedestrians. The fundamental design principle of this concept plan is to enhance the Eastern Urban Center and distinguish its districts through its composition of street trees. In addition, the use of native California trees is encouraged with this plan, to link the *EUC*'s identity with the surrounding region, although other factors such as disease resistance, aesthetics, and maintenance have contributed to the representative species recommended here.
- b. The street tree concept plan is based on the designated landscape zones and other unique functions that some of the streets serve throughout the *EUC*, see Exhibit III-63 through III-64e (Street Tree Plan) for these zones. In particular, this concept plan identifies five distinct types of street trees that are assigned to corresponding streets. Each street tree type has several representative species that should be combined to create a unique setting along their designated streetscapes. These five types are briefly described below, and include several representative species. In addition, this concept plan recommends using selected palm trees at all intersections within the *EUC*, to help define these important urban features and provide a unifying element.
- c. Type 1 trees are located along A Street in the office and civic landscape zones. These areas will have more massive buildings along a structured sequence, and the street itself will have a relatively large cross section. As a result, Type 1 trees must be

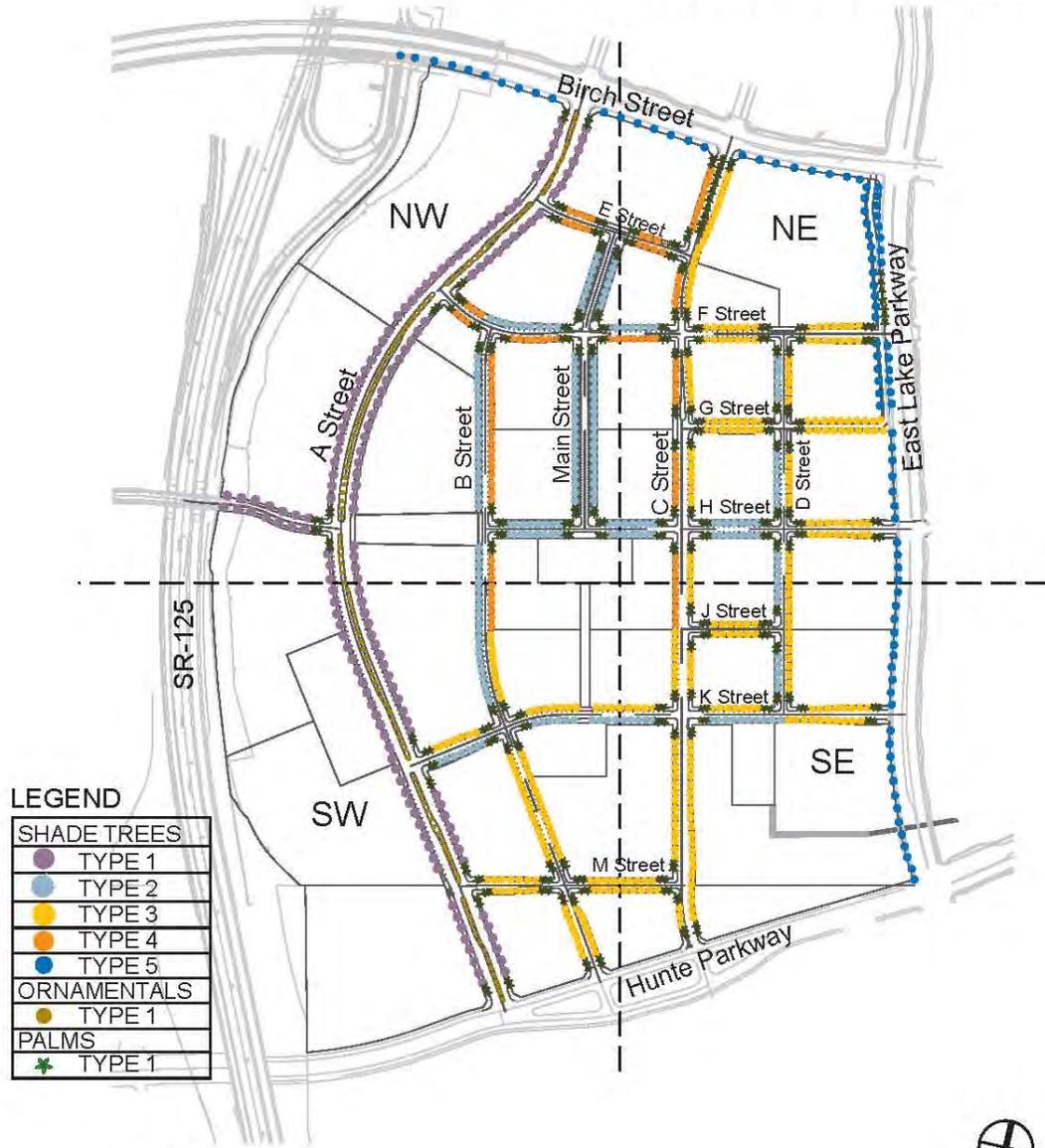
appropriately scaled to complement the other streetscape elements. These trees should be large and evergreen, and have relatively dark leaf color and bold overall shape and form. Representative species include:

- i. *Magnolia grandiflora* 'Majestic Beauty' - Majestic Beauty Hybrid Southern Magnolia
 - ii. *Quercus ilex* - Holly Oak
 - i. *Podocarpus gracilior* - Fern Pine
- d. In addition to the Type 1 trees along A Street, ornamental trees are used in the street's medians. The median trees should contrast with those that line A Street, by providing visual interest with flowers and a more horizontal or spreading form. Representative species include:
- i. *Cercidium x 'Desert museum'* - Blue Palo Verde.
 - ii. *Cercis canadensis* 'Forest Pansy' - Eastern Redbud or
 - iii. *Cercis occidentalis* - Western Redbud (native)
 - iv. *Pistache chinensis* - Chinese Pistache
 - v. *Tabebuia chrysotricha* - Pink Trumpet Tree
- e. Type 2 trees are located along the Circuit Walk and Main Street, which will have broad sidewalks and intense pedestrian activity. Trees here should provide a distinguished setting and refreshing environment that contrasts with surrounding buildings and pavement. Larger trees should be planted along these streets, especially Main Street, to accommodate activity underneath such as outdoor dining and easy movement to the sidewalk from cars parked on the street. Trees should provide ample shade in the summer, and should include deciduous trees to provide sun in the winter months where needed. Along the Circuit Walk, the trees are to be planted in double rows, and should have visual interest such as showy bark, prominent flowers, or interesting form. These trees should also have arching forms, to create an arcade-like feel that provides continuous enclosure for the Circuit Walk streetscape. Representative species include:
- i. *Quercus agrifolia* - Coast Live Oak (native)
 - ii. *californica* - California Sycamore (native)
 - iii. *Platanus x acerifolia* 'Columbia' - London Plane Tree
 - iv. *Tipuana tipu* - Tipu Tree
- f. Type 3 trees occur in the residential areas of the EUC. Most of the street trees in this area should be evergreen and have different shape and form compared with other EUC landscape zones. These trees may be smaller, and should also have an arching growth habit to provide a more intimate canopy, to be in scale with the narrower residential streets. The leaf morphology and branching patterns of these trees should form a light and airy canopy, to allow filtered sunlight onto the sidewalks below. These trees should also have showy flowers that occur throughout the year, to provide ongoing visual interest for residents. Representative species include:

- i. *Geijera parviflora* - Australian Willow
 - ii. *Koelreutaria paniculata* - Goldenrain Tree
 - iii. *Koelreutaria bipinnata* - Chinese Flame Tree
 - iv. *Jacaranda mimosifolia* - Jacaranda
- g. Type 4 trees are planted in the commercial/mixed use area of the *EUC*, which is near Main Street and the Circuit Walk. It is important that Type 4 trees are distinguishable from those along Main Street and the Circuit Walk, while still having similar, shape, form and size for appropriate scale. For instance, these trees may exhibit varied flowering times, a slightly larger size, or different bark color than those planted along the Circuit Walk and Main Street. Deciduous trees with interesting fall color should also be considered in this zone. Representative species include:
- i. *Liquidambar styracifula* 'Palo Alto' - Palo Alto Sweet Gum
 - ii. *Agonis flexuosa* - Peppermint Tree
 - iii. *Ulmus parvifolia* 'True Green' - Tree Green Hybrid Chinese Elm
- h. Type 5 trees are planted along the perimeter streets of the *EUC*. These trees should provide a strong and consistent street edge adjacent to the meandering sidewalk. For the sidewalk, a different type of tree should be planted in alternate fashion along both sides. The trees along the sidewalk should have showy flowers or other visually interesting features for pedestrians. Representative species include:
- i. *Hymenosporum flavum* - Sweet Shade
 - ii. *Tristania conferta* - Brisbane Box
 - iii. *Eucalyptus torquata* - Coral Gum
- i. Palm trees are the unifying street tree throughout the *EUC*. Palms should be used at all intersections, to provide the visual consistency that knits together the *EUC* street trees. Like other street trees, palm trees provide a strong vertical element, contribute a green amenity, and define the street edge, but their limited canopy area at intersections visually opens up the space. This also provides sight lines needed for safety of motorists and pedestrians. Palm trees should also be used along the C Street median, where the BRT Route is proposed, to provide the vertical clearance needed for large buses along C Street. Representative species include:
- i. *Phoenix dactylifera* - Date Palm
 - ii. *Archontophoenix cunninghamiana* - King Palm
 - iii. *Syagrus romanzoffiana* - Queen Palm

Photo examples of a selection from each type follow as Exhibits III-65a to III-65i (Trees).

Street Tree Plan



KEY PLAN

Note: See Quadrant Enlargements

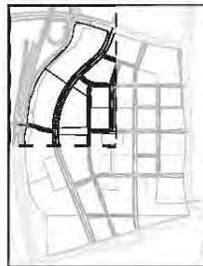


Exhibit III-63

Street Tree Plan Northwest Quadrant

LEGEND

SHADE TREES	
	TYPE 1
	TYPE 2
	TYPE 3
	TYPE 4
	TYPE 5
ORNAMENTALS	
	TYPE 1
PALMS	
	TYPE 1



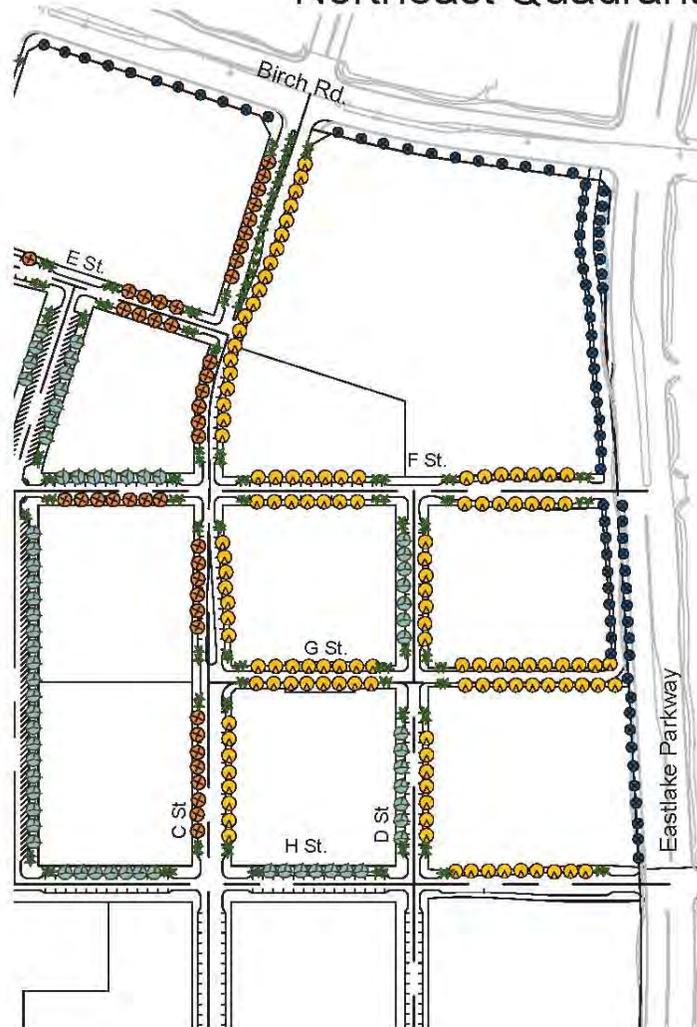
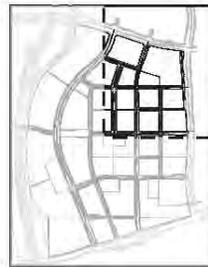
NOTE: TREE 'TYPES' MAY INCLUDE MORE THAN ONE SPECIES OR VARIETY

Exhibit III-64a

Street Tree Plan Northeast Quadrant

LEGEND

SHADE TREES	
	TYPE 1
	TYPE 2
	TYPE 3
	TYPE 4
	TYPE 5
ORNAMENTALS	
	TYPE 1
PALMS	
	TYPE 1



NOTE: TREE 'TYPES' MAY INCLUDE MORE THAN ONE SPECIES OR VARIETY



Exhibit III-64b

Street Tree Plan Southwest Quadrant

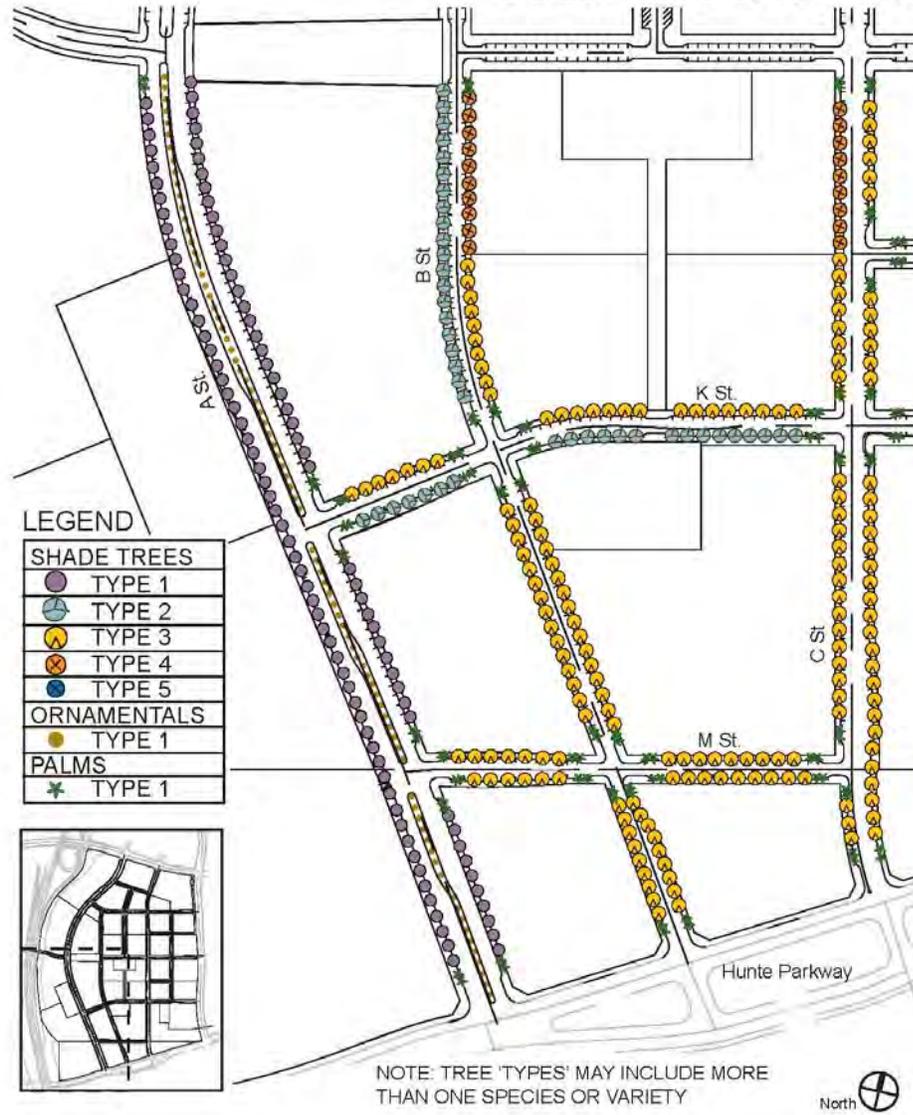
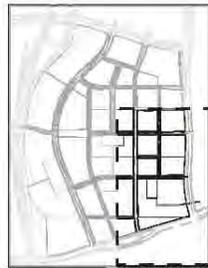


Exhibit III-64c

Street Tree Plan Southeast Quadrant

LEGEND

SHADE TREES	
	TYPE 1
	TYPE 2
	TYPE 3
	TYPE 4
	TYPE 5
ORNAMENTALS	
	TYPE 1
PALMS	
	TYPE 1



NOTE: TREE 'TYPES' MAY INCLUDE MORE THAN ONE SPECIES OR VARIETY



Exhibit III-64d

Coast Live Oak
Quercus agrifolia



TREES



Exhibit III-65a

Magestic Beauty Hybrid Southern Magnolia

Magnolia grandiflora 'Magestic Beauty'



TREES



Exhibit III-65c

Exhibit III-65b

True Green Hybrid Chinese Elm

Ulmus parviflora 'True Green'



TREES



Exhibit III-65d

Chinese Pistache

Pistache chinensis



TREES



Exhibit III-65e

Brisbane Box
Tristania conferta



TREES



Exhibit III-65f

Palo Alto Sweet Gum

Liquidambar styracifula 'Palo Alto'



TREES



Exhibit III-65b

Date Palm
Phoenix dactylifera



PALM TREES



Exhibit III-65i

03.12.003 Planting Performance Standards

- a. Planting shall be installed, irrigated, and maintained per the City Landscape Design Manual, dated November 1994 and any subsequent amendments thereto.
- b. Planting material and design shall adhere to the Otay Ranch Overall Design Plan, dated March 1995, as appropriate for an urban center.
- c. The Birch Road, EastLake Parkway and Hunte Parkway streetscapes will incorporate the ranch theme designated in the Plan. A specific tree shall run the length of each edge of the *EUC*.
- d. The SR-125 streetscape will also incorporate elements of the ranch theme, but will feature a variety of trees that form screens and formal patterns, when appropriate. Views into the *EUC* from SR-125 will be created at key points.
- e. Planting shall help to differentiate and accentuate the project landscape zones.
- f. Each street within the *EUC* will have distinctly different dominant tree species and/or patterns. Typically street trees shall be spaced from 25 feet apart to 40 feet apart.
- g. Gateways and transitions from each zone will feature ornamental or special plantings.
- h. Planting shall be maintained in accordance with the responsibilities set out in an approved Landscape Master Plan.

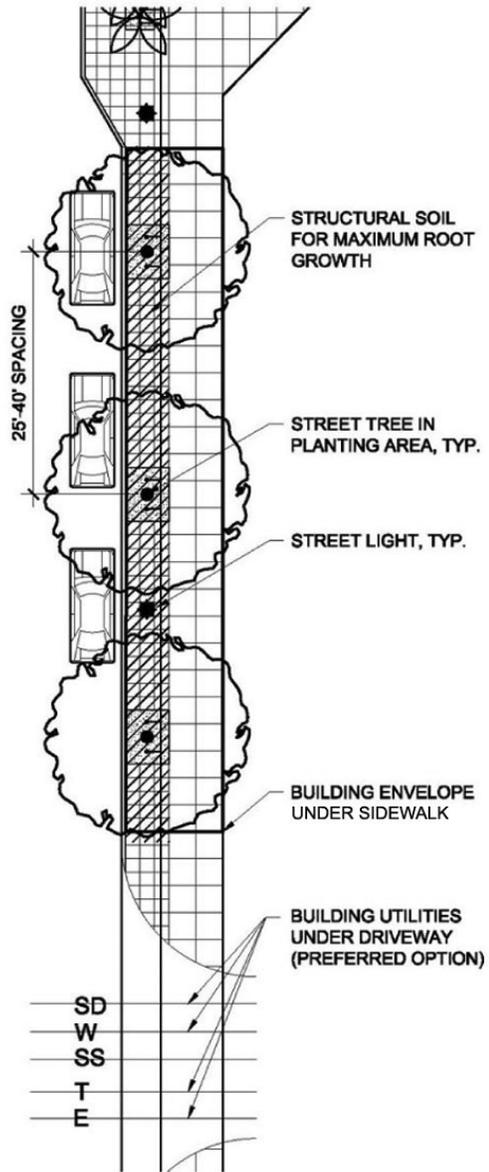
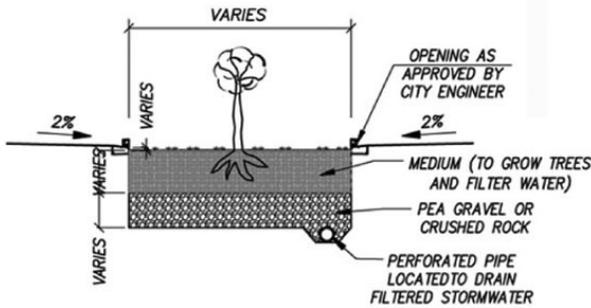
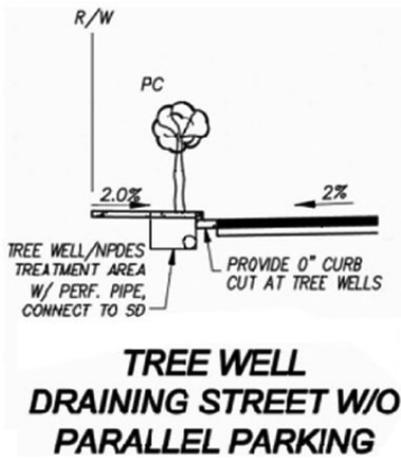
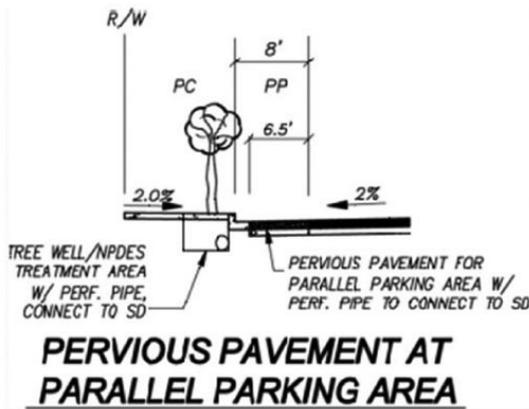
03.12.004 Tree Planting - Tree Pit and Hardscape Construction. Refer also to Exhibit III-66 (Planting Areas & Root Zones) for details.

- a. Continuous planting and/or root zones should be encouraged to create larger areas for tree roots to spread. Coordinate placement of utilities, driveways, light poles, bus stops and hydrants to minimize interference with trees.
- b. Structural soil, which consists of about 80% small, triangular stones and about 20% loam and water retention material, should be utilized to prevent soil compaction, preserve large air spaces and help ensure oxygen supply to the roots. Exclusive of the root ball, the soil type should be consistent throughout the planting and rooting zones.
- c. In certain areas, a subsurface drainage system may be recommended to remove excess water from root zones, facilitate proper air exchange and prevent the trees and planting from over-watering and eventual drowning.
- d. Pavers can be utilized over the root zone to facilitate air exchange and water recharge.
- e. Planting zones around street trees may consist of one or more of the following: groundcover, shrub plantings, decomposed granite, cobble and/or pavers (without

grouted joints) or tree grates.

- f. In tree pits with grates, root balls can be set lower than bottom of sidewalk paving to allow root growth under hardscape.
- g. Root barriers should be utilized to help to retard shallow roots, but allow large anchor roots to develop below. Root barriers may not be covered by soil or mulch, as roots will grow over the top of the barrier.

Planting Areas & Root Zones



Note: Refer also to Street sections in this Chapter for additional detail.



Eastern Urban Center OTAY RANCH

Source: WRT
Urban Design: RTKL
Cinti Land Planning
San Diego, CA (619) 223-7408

Exhibit III-66

Urban Landscape Character



Grove



Farmers Market



Planting under a covered walk



Eastern Urban Center
OTAY RANCH



Exhibit III-67

Urban Landscape Character



A Combined Landscaped Driveway & Pedestrian Paseo



Natural Plantings for Relief



Tropical Planting in a Courtyard Environment



**Eastern Urban Center
OTAY RANCH**

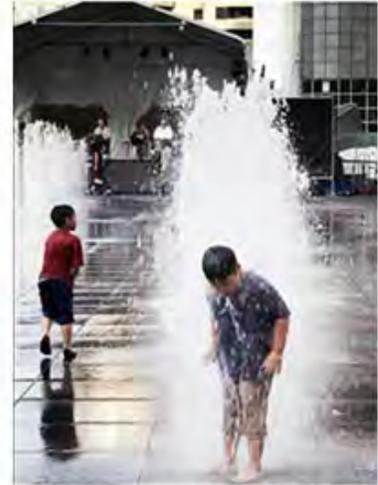


Exhibit III-68

Urban Landscape Character



Defining an Entry with Planting and Trellis



Fountains that entertain



Providing quiet places to assemble



Places to people watch



Special Hardscape treatments can be used for seamless transitions



Eastern Urban Center
OTAY RANCH



Exhibit III-69

Urban Landscape Character



An Urban Grove



Balconies, planters, and awnings add interest to walkways



Plazas & streets are blended as pedestrian friendly spaces



Street furniture is a place to read



Opportunities for evening strolls



Providing surprise plantings



Water features to cool a summer day



**Eastern Urban Center
OTAY RANCH**



Exhibit III-70

03.12.005 Review and Approval Process

Landscape plan submittals shall be reviewed in accordance with Chapter 04 herein. A landscape master plan shall be required as specified therein.

03.12.006 Parks, Plazas & Other Open Space

- a. Parks: The Urban Parks, Recreation, Open Space & Trails Plan component of the SPA Plan includes a full description of the public park provisions.

- a. Open Space Requirements

The minimum open space requirement for residential uses shall be 200 square feet per dwelling unit.

The term “open space” refers to any areas with a minimum dimension of 60 square feet (6'x10') and devoted to the following common, private, or public uses: patio, porch, balcony, deck, garden, playground, plaza, swimming pool, sports court/field, recreation room, gym, spa, community room, lobby (with recreational uses), open corridor, cultural arts, pedestrian amenities, lawn/turf, hardscape, pond, fountain, atrium, sunroom, theater, amphitheater, band shell, gazebo, picnic area, shelter, terrace, roof areas designed to accommodate recreational and leisure activities, or similar passive or active recreational/leisure use or facility that is not used for enclosed dwelling unit floor area or commercial space. Open spaces may also appear in the form of less apparent uses devoted to water quality features like storm water filtration, storm water filtration areas which are used for recreation purposes, vegetated swales capturing building runoff, detention basin or cascading or daylighting of water. Common usable open space may be joint use facilities shared between projects or buildings to meet the needs of future residents.

The open space requirements for non-residential development shall be as provided for in the requirement for plazas, as described in the Urban Parks, Recreation, Open Space & Trails Plan.

Main Street Town Square Concept



**Eastern Urban Center
OTAY RANCH**

Source: RTKL
Cinti Land Planning
San Diego, CA (619) 223-7408

Exhibit III-71

Main Street Sidewalk Cafes



Pedestrian travelway protected from encroachment by cafe seating areas



A street activated by outdoor cafes and active use median



Dining Area extends from inside building to cafe area on sidewalk



Dining Area secured by attractive railing



**Eastern Urban Center
OTAY RANCH**



Exhibit III-72

03.12.007 Street Furniture

Each district (neighborhood) should have a distinctive dominant street furniture design theme. Street furniture in public rights-of-way shall be approved by the Zoning Administrator and the Director of public Works using examples in this Code as a guide. Photo examples of appropriate street furniture are provided as Exhibits III-73 through III-77 (Street Furniture).

Street Furniture Performance Standards:

- a. All furniture shall be of contemporary style.
- b. Furniture material and coating shall be metal, polystyrenel, or other durable material.
- c. Wood used as a material for street furniture is not encouraged.
- d. Furniture should use recycled materials when possible.
- e. All metal furniture shall have a coating that: resists rusting; resists corrosion; resists dulling of color due to ultraviolet; and can be easily cleaned and maintained.
- f. Seat dividers or arm rests should be used to discourage sleepers and skateboarders.

03.12.008 Public Art

Public art will be provided to enhance pedestrian orientation. Installations which are also functional forms, such as unique paving, street furniture, bike racks, fountains, major entry signs, etc. are specifically appropriate. Refer to Exhibit III-78 (Public Art) for examples of functional public art. Locations for public art installations are shown on the District Design Framework Plans and in the Urban Parks, Recreation, Open Space and Trails Plan. Implementation of the requirement shall be enforced through the project Design Review process, or the park design review process. Should the City adopt a master plan which includes a requirement for public art, the public art provided by the master developer in the EUC shall be given a credit towards the requirement in the applicable fee program.

Artwork in the EUC shall be consistent with the following:

- a. Artwork should be incorporated in EUC in public spaces in things such as benches, bollards, sculpture, bike racks, transit shelters, paving, etc.
- b. Artwork shall be placed in designated areas in which it will be highly visible to the public.
- c. All public artwork shall be functional to the site in which it is placed and coincide with the overall design theme of the district in which it is placed through the use of materials, textures, and colors.

- d. Public art includes art within lobbies or foyers of private buildings.
- e. Some public art pieces shall be temporary to allow for new artwork to replace them over time.

Street Furniture

Seating Examples



Note: A variety of street furniture is envisioned to add distinct character to the neighborhoods.



Eastern Urban Center
OTAY RANCH



Exhibit III-73

Street Furniture

Bike Racks



Other materials and shapes are also permitted.



Eastern Urban Center
OTAY RANCH



Exhibit III-74

Street Furniture Trash Receptacles



Note: A variety of different Trash Receptacles is envisioned to define neighborhoods. Business or project logos may also be used on them.



**Eastern Urban Center
OTAY RANCH**



Exhibit III-75

Street Furniture

Bollards



Bollards can be removable or mounted permanently as required by the application. The examples above illustrate the variety of shapes available. Other materials, such as stone, and concrete are also suitable.



Eastern Urban Center OTAY RANCH



Exhibit III-76

Street Furniture

Tree Grates



Exhibit III-77

Public Art Examples



As a Railing Detail



As a Park Feature

As paving



**Eastern Urban Center
OTAY RANCH**



Exhibit III-78

03.13.000 Lighting

The EUC project area has been divided into Lighting Zones in order to differentiate various locations with varying lighting designs. The Lighting Concept Plan is provided as Exhibit III-79. Each lighting district (neighborhood) should have a dominant lighting theme/character. Lighting in public rights-of-way shall be approved, as a part of the street and park improvement plans using lighting examples as a guide. Lighting on private property shall be approved by the Zoning Administrator. No city-standard street lights are required within the EUC except for lighting on arterial roadways.

03.13.001 Standards

- a. All non-standard street lighting shall be approved by the director of public works.
- b. Park lighting shall be consistent with the Chula Vista Landscape Manual and approved by the Director of Engineering.

03.13.002 Lighting Performance Standards

- a. All lighting shall be of contemporary styling.
- b. Street Lights shall be placed on all public streets within the EUC.
- c. Lighting design should be designed to minimize glare, light trespass, energy conservation, and to maintain dark skies. The light designers should consider utilizing automatic controls systems to eliminate excessive light during non-active hours of site operation.
- d. Full cut-off fixtures, mounting heights, and shielding should be utilized to effectively control glare and light trespass.
- e. Any exterior lighting designs shall take into account all exterior lighting sources.
- f. Recommended light level guidelines and uniformity ratios established by the Illumination Engineering Society of North America (IESNA), should be considered when determining appropriate lighting design solutions.
- g. All lights mounted on poles of 8 ft. or greater in height shall be directed down. The light source shall be shielded so that it will not be visible from any adjacent real property.
- h. Pole heights should not exceed 35 in height and no less than 25 on roadways to avoid glare problems.
- i. Architectural lighting is required on all tower, iconic, and gateway buildings, and should be provided on all buildings, to illuminate architectural features.

- j. Lighting design for parks shall take into consideration its impact on adjacent residential uses.

03.13.003 Implementation

The lighting concepts shall be implemented in one of two ways: during design review for the first project in any district; or, by approval of a lighting program, which must cover at least an entire lighting district. The lighting program shall be approved by the Director of Building and Planning and the City Engineer.

Lighting Concept Plan

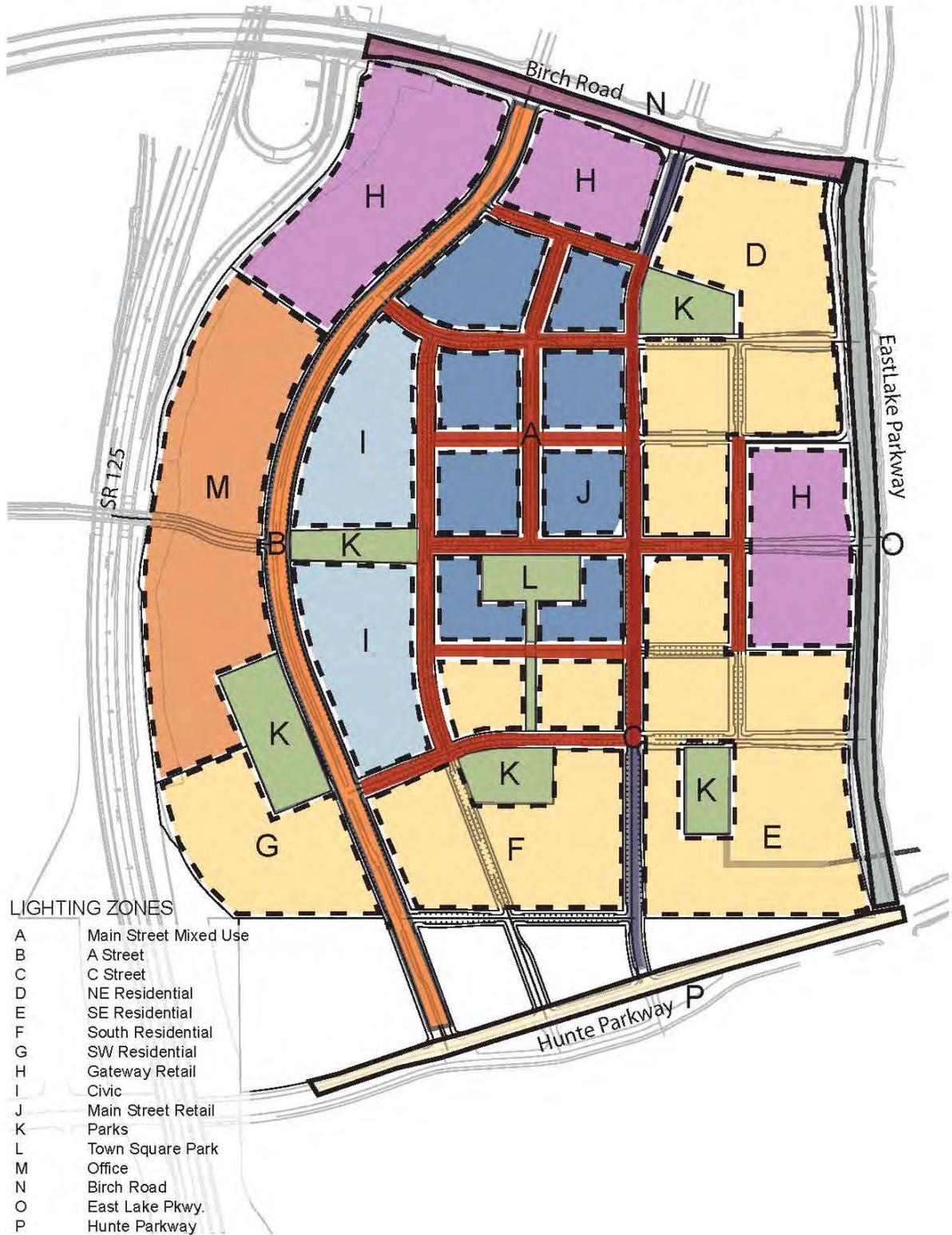


Exhibit III-79

03.14.000 Signing**03.14.001 Conceptual Plan**

The conceptual plan for signing within the EUC is illustrated in Exhibit III-80 (Signage Concept Plan) which depicts two types of neighborhood gateway/entry monuments which will include signage and eight signage districts. All signs, placed either within the EUC or off-site, shall be approved through the adoption of a Sign Program for the respective project or district, consistent with Section 19.60.210 CVMC. All sign programs, or modifications to same, shall be approved by the Master Developer prior to approval required by the city. Signs approved by the Zoning Administrator as a part of a comprehensive sign program shall not require a sign permit.

03.14.002 Permitted Signs

Signs shall be regulated pursuant to Chapter 19.60 CVMC except as modified by the provisions herein. Prior to adoption of the Sign Program the following signs shall be permitted: temporary construction and sales signs, and any sign approved by Design Review. Marketing signs and billboards, including orientation to SR-125 and within CFD maintained slopes shall be permitted during the entire sales and marketing period for the EUC.

The use of creative signing will be encouraged in the EUC. Examples of creative signs include the use of graphic screens or displays over empty storefront spaces, identifying or thematic signs on street furniture such as trash receptacles, or the use of pavement signs in parking lots to add visual interest and as a means of promoting both businesses and special events or programs. Approval for these sign options shall be established in an approved Sign Program or Design Review.

03.14.003 Prohibited Signs

All signs and lighting not expressly permitted herein, or by an approved comprehensive sign program, shall be prohibited in all districts including, but not limited to the following:

- a. Vehicle signs (when parked or stored on property to identify a business or advertise a product).
- b. Portable signs (except where permitted in by these regulations).
- c. Off-site signs (except temporary subdivision, project marketing, or real estate signs, and political signs).
- d. Signs within the public right-of-way (excluding those required by a governmental agency and project/special event banners) unless approved with a use conducted within the public ROW. No sign shall be placed, erected or constructed on a utility

pole, traffic device, traffic sign, warning sign, or so as to impede access to any public improvement.

- e. Signs located on public property except as may be specifically permitted by an approved Comprehensive Sign Program, or required by a governmental agency.
- f. Signs within the public right-of-way prohibited by the Streets and Highway Code (Sec. 101 et. seq. and Sec. 1460 et. seq.), the Vehicle Code (Sec. 21400 et. seq.) and the Public Utilities Code (Sec. 7538 et. seq.).
- g. Signs blocking doors or fire escapes.
- h. Inflatable advertising devices of a temporary nature, including hot air balloons (except for permitted special events).
- i. Signs displayed as, which purport to be, are an imitation of, or resemble official traffic warning devices or signs, that by color, location or lighting may confuse or disorient vehicular or pedestrian traffic. This does not include traffic or directional signs installed on private property to control on-site traffic.
- j. Roof signs.
- k. Advertising structures (except as otherwise permitted in this section).
- l. Statuary (statues or sculptures) advertising products or logos of the business located outside of the structure that houses the business.

03.14.004 Design Standards

Each sign shall be designed with the intent and purpose of complementing the architectural style of the main building or buildings, or type of business on the site. Signs located on institutional or community purpose sites, but in a predominantly residential area, shall take into consideration compatibility with the residential area to the extent possible.

a. Sign Copy

The name of the business, use, service and/or identifying logo shall be the dominant message on the sign. The inclusion of advertising information such as lists of products (more than one product), is prohibited.

b. Relationship to Streets

Signs shall be designed so as not to obstruct any pedestrian, bicyclist, or driver's view outside of sight visibility unless otherwise approved by the city engineer.

c. Design Review & Approval

Modifications or refinements to the sign plan, including sign locations, size, style, materials, lettering size and style, and any other information necessary to adequately review the proposal shall be prepared and submitted for review and approval by the Zoning Administrator prior to construction.

03.14.005 Performance Standards

- a. Standard Chula Vista street name signs need not be used at all street intersections, if an alternative street sign is submitted and approved by the Zoning Administrator and the Director of Public Works.
- b. Variations in signage color, shape and content will help delineate various neighborhoods, districts and zones within the project.
- c. All custom signage elements shall be of contemporary styling.
- d. Wayfinding elements to direct visitors to important public spaces and services such as the Civic Center, the Otay Center Promenade, parking facilities, and parks shall be allowed.
- e. All elements shall be easily legible, concise and clear
- f. All elements shall be made of durable and commonly used materials so that they may be easily duplicated, refurbished or replaced in-kind in the future.
- g. All wayfinding elements shall be covered in an anti-graffiti coating.
- h. The Chula Vista Regional Trail shall be clearly demarcated along its entire length through a combination of special signage and/or paving elements.
- i. Festive and/or seasonal banners shall be not permitted without an approved sign program, except they may be permitted at any time in the Mixed Use and the Mixed Use/Civic districts.
- j. City engineer must approve any alternative to an adopted city standard.

03.14.006 Gateway Signing:

The Land Use Element of the City's General Plan identifies the portion of Birch Road adjacent to the EUC as a City gateway. To reinforce the primacy of Birch Road as an entry to the City from the SR125, a City Gateway sign will be placed in the median of Birch Road to identify this important gateway. (Refer to Exhibit III-80, Signing Concept.) The City's General Plan recommends that the City prepare Gateway Master Plans for each of the gateways identified in the General Plan, including assuring that any signage designs conform to applicable entryway / gateway design guidelines and standards. The provisions of this plan

are not intended to override any gateway master plan once adopted, instead it provides preliminary guidance to be considered by the City in developing any future master plan.

- a. Recommended design elements for the Birch Road gateway could include the following:
 - i. Low profile design with enhanced landscaping to draw the eye while at the same time not competing with signage text or message.
 - ii. Use of architectural lighting to enhance the sign at night.
 - iii. Use of durable materials with local context and significance.
 - iv. Contextual, yet unique design which address complements the surrounding environment (both built and open space), yet is recognizable as part of the City-wide gateway program
- b. It is expected that the City will be preparing and adopting it's gateway master plan prior to the applicant constructing the sign in the Birch Road median and that the adoption of that plan will drive the timing of the sign installation. Once the City's plan has been adopted, the implementation of the gateway sign shall be as prescribed in the next EUC sign program (overall or district) submitted for approval. Until the City has adopted it's gateway master plan, the application, processing, and approval of development applications and sign programs within the EUC shall not be restricted.

13.14.007 Implementation

The signing concepts shall be implemented in one of two ways: during design review for the first project in any district; or, by approval of a signing program, which must cover at least an entire signing district. The signing program shall be approved by the Director of Building and Planning and the City Engineer. Signs consistent with signing program shall also require a sign permit.

Signage Concept Plan

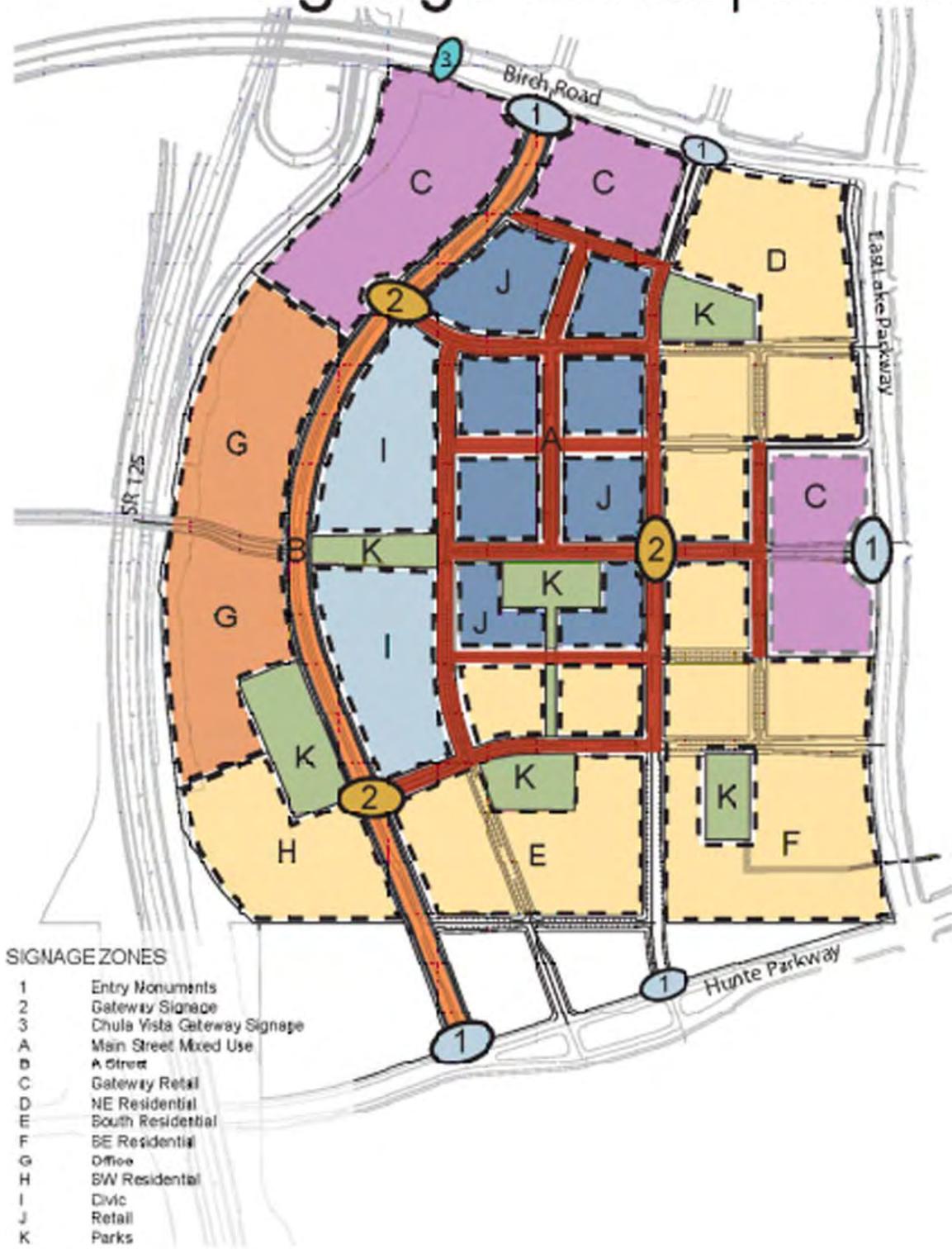


Exhibit III-80

Signage

Major Entry at Birch Road

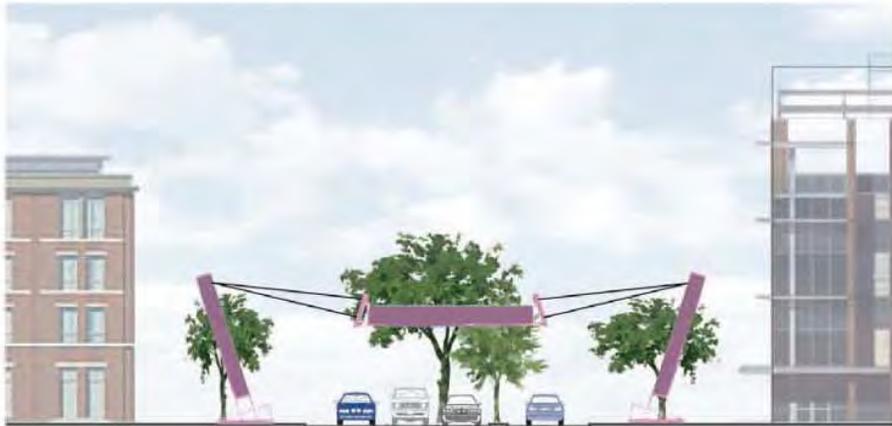


Eastern Urban Center
OTAY RANCH



Signage

Main Street Gateways



Eastern Urban Center OTAY RANCH



9/15/06
Exhibit III-82

Signage

Major Entry at EastLake Pkwy.



Eastern Urban Center
OTAY RANCH



9/15/06
Exhibit III-83

03.15.000 Parking Standards

03.15.001 Parking Principles:

- a. Encourage a “Park once, walk further” environment.
- b. Parking should not be over-prescribed.
- c. Total parking demand will consider walkability, transit and mixed use reductions
- d. Shared Parking, when feasible, should be mandatory
- e. Parking should be actively managed to maximize efficiency

03.15.002 Parking Requirements - General:

Parking space requirements shall be as provided for herein, or as ~~determined by~~ consistent with an approved Parking Management Plan. The Parking Management Plan shall consider shared parking opportunities and time of day and day of the week differences in peak parking demand to accurately project the total amount of parking required to meet total demand at any one time. All available on-street parking spaces shall be counted towards satisfying a project’s total required parking.

The Director of Development Services shall approve and adopt the Parking Management Plan, which shall incorporate the parking rates adopted by City Council Ordinance _____ and reflected in Table III-A herein. Once the Parking Management Plan is adopted, all subsequent revisions to parking rates in Table III-A, herein and in the Parking Management Plan, shall require prior review and approval by the Planning Commission until such time as the Parking District Council transfers from the Master Developer to the Community Association. Following this transfer, further revisions to parking rates shall be approved by the Director of Development Services.

03.15.003 Parking Rates:

~~Reduced~~ ~~Parking Base~~ Rates: Table III-A (Parking Rates) for the EUC are established based on the following national parking standards. These standards are intended to be implemented in conjunction with the approved Parking Management Plan. In calculating required parking for a single use, the highest number of parking spaces (weekday vs. weekend) shall be used. For mixed use, multiple use or shared use scenarios the sum of the required spaces for all uses shall be determined for both the weekday and weekend scenarios, with the larger of the two governing.

For uses not identified in the table below, parking rates shall be determined by using the provisions outlined in General Note 3 of Table III A with appropriate adjustments made by a registered traffic engineer to account for the mixed use and transit-oriented nature of the EUC.

**TABLE III A
PARKING RATES**

LAND USE	PARKING RATES	
	WEEKDAY	WEEKEND
COMMERCIAL CENTER		
Community Shopping Center (<400,000 sq.ft.) ^c	3.6/ksf GLA	4/ksf GLA
COMMERCIAL (Stand-Alone Land Uses)		
Retail		
Bank	5/ksf GLA	18/ksf GLA
Restaurants ^{a, c}		
Fine/Casual dining (Quality Restaurant)	18/ksf GLA	20/ksf GLA
Family Restaurant (High-Turnover/Sit-down Restaurant)	10.5/ksf GLA	15/ksf GLA
Fast-Food Restaurant, including coffee shops with or without drive-through window ^b	15/ksf GLA	14/ksf GLA
Nightclubs	16.5/ksf GLA	19/ksf GLA
Cineplex		
Multiplex movie theater	0.2/seat	0.29/seat
Performing Arts Theaters and Arenas		
Performing Arts Theater (Live Theater)	0.37/seat	0.4/seat
Sports arenas, authoriums, assembly halls	0.29/seat	0.29/seat
Health Clubs and Entertainment/Party Venues		
Health/Fitness Club	7/ksf GLA	5.75/ksf GLA
Bowling Alleys	5/lane	5/lane
Hotels		
Hotel	1.04/room	0.97/room
OFFICE		
General Office (<150,000 sq. ft.)	3.33/ksf GLA	0.38/ksf GLA
General Office (>150,000 sq. ft.)	2.8/ksf GLA	0.353/ksf GLA
Medical/Dental Office	5/ksf GLA	0.38/ksf GLA
Government Office Building	6.13/ksf GLA	0.6/ksf GLA
RESIDENTIAL		
Apartment	1.65/du	1.65/du
Residential condominium/townhouse		
< 4 bedrooms	1.85/du	1.85/du
5+ bedrooms	1.85/du +0.5/bedroom >4	1.85/du +0.5/bedroom >4
Senior adult housing-attached ^d	0.66/du	0.66/du
Assisted living	0.33/bed	0.33/bed

continued on following page with footnotes

TABLE III A (continued)
PARKING RATES

LAND USE	PARKING RATES	
	WEEKDAY	WEEKEND

RESIDENTIAL (Continued)

Live-Work Units (Commercial component <50% of total GFA)	1.85/du	1.85/du
Live-Work Units (Commercial component <50% of total GFA)	1.85/du + 30% of corresponding rate for non-residential use	1.85/du + 30% of corresponding rate for non-residential use

Footnotes:

- a For enclosed proprietary outdoor dining spaces, up to 200 square feet, is exempt from minimum parking requirements. Above 200 square feet, a minimum of 5 spaces per ksf should be provided. All sidewalk cafes within public right-of-way are exempt from minimum parking requirements
- b Fifteen (15) spaces minimum.
- c For shopping centers with dining and entertainment land uses, the ULI recommends the following methodology for determining the appropriate parking rate to apply:

If Dining/Entertainment Uses in Shopping Center equals:	Parking Requirement
Less than 10% of GLA	Apply shopping center parking rates, as-is, to the total GLA
10%-20% of GLA	Increase parking rate by 0.03 for every 1% above 10%
Greater than 20% of total GLA	Use the stand-alone dining and entertainment land use rates for the dining and entertainment land use GLA and the shopping center rate for the remaining GLA. Conduct a parking study.

- d Senior adult housing at market rate (not affordable)

General Notes:

- 1 ksf GLA = 1,000 square feet of gross leasable area. Gross leasable area is the amount of floor space available to be rented in a commercial property. It is the total floor area designed for tenant occupancy and exclusive use (including any basements, mezzanines, or upper floors), but not including areas such as utility, roof access or fire service rooms accessible from the outside of the lease space.
- 2 du = dwelling unit
- 3 For any land use not listed in above, the developer will have options, such as using a parking ratio published by ITE, ULI or the City of Chula Vista Municipal Code for similar land use, using the parking ratio obtained from a project-specific study, a case study, or conducting a parking survey at an existing site to determine an appropriate parking rates.

Projects requesting parking at a higher rate than those shown in the table above are required to submit a parking study, approved by the parking district as part of their Design Review application.

03.15.004 Design Standards

The following types off-street parking facilities shall be permitted:

- a. Surface Lot
- b. Tuck-under / Direct Access Parking
- c. Subterranean / Podium Parking
- d. Central (Embedded or Wrapped) Parking Structure
- e. Parking Garage
- f. Tandem Parking

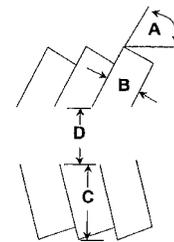
Table III-B
Parking Stall Dimensions

Standard Parking Space	8' 3" x 18'
Retail/Restaurant Space	8' 6" x 18'
Spaces adjacent to columns/walls (one side)	+ 1 foot
Spaces adjacent to columns/walls (two sides)	+ 1 foot
Setback Reductions	Reduced by 3" every 1' column is set back from aisle
Space Parallel to Aisle	8' x 24'

Table III-C
Parking Aisle Dimensions

EASTERN URBAN CENTER PARKING TABLE (DEVELOPMENT STANDARDS)									
A	B	C	D		A	B	C	D	
			One Way	Two Way				One Way	Two Way
0°	8' 3"	24	12	20	60°	8' 3"	19' 9"	18	22
	8' 6"	24	12	20		8' 6"	19' 10"	18	22
	8' 9"	24	12	20		8' 9"	20'	18	22
	9'	24	12	20		9'	20' 1"	18	21
	9' 3"	24	12	20		9' 3"	20' 3"	18	21
	9' 6"	24	12	20		9' 6"	20' 4"	18	20
20°	8' 3"	13' 11"	12	20	70°	8' 3"	19' 9"	22	24
	8' 6"	14' 2"	12	20		8' 6"	19' 10"	22	24
	8' 9"	14' 5"	12	20		8' 9"	19' 11"	21	24
	9'	14' 7"	12	20		9'	20'	20	23
	9' 3"	14' 10"	12	20		9' 3"	20' 1"	19	23
	9' 6"	15' 1"	12	20		9' 6"	20' 2"	19	22
30°	8' 3"	16' 2"	12	20	80°	8' 3"	19' 2"	24	24
	8' 6"	16' 4"	12	20		8' 6"	19' 2"	24	24
	8' 9"	16' 7"	12	20		8' 9"	19' 3"	23	24
	9'	16' 10"	12	20		9'	19' 3"	22	23
	9' 3"	17'	12	20		9' 3"	19' 4"	21	23
	9' 6"	17' 3"	12	20		9' 6"	19' 5"	20	22
45°	8' 3"	18' 7"	13	20	90°	8' 3"	18'	24	24
	8' 6"	18' 9"	13	20		8' 6"	18'	24	24
	8' 9"	18' 11"	13	20		8' 9"	18'	23	24
	9'	19' 1"	13	20		9'	18'	22	23
	9' 3"	19' 3"	13	20		9' 3"	18'	21	23
	9' 6"	19' 5"	13	20		9' 6"	18'	20	22

- A Parking Angle
- B Stall Width
- C Stall to Curb
- D Aisle Width



Minor reductions or modifications in design standards may be permitted subject approval of the City Engineer

03.15.005 Parking Facility Performance Standards

- a. Avoid curb-cuts along Main Street, relying on side streets for access to parking

facilities.

- b. Position on-site parking facilities to minimize their visual impact on the street. Parking facilities should generally be embedded (located interior to the block) and should avoid interrupting storefront continuity along principal shopping streets.
- c. Parking garages that are adjacent to the street right-of-way of the EUC's primary grid of public streets shall not be located on opposite sides of the street right-of-way for a distance of more than 200 feet or 50% of the length of the block, whichever is less. The EUC primary grid of public streets are A, B, C, F and K Streets.

03.15.006 Architectural Design

- a. Design parking structures that are compatible with adjacent buildings in terms of scale, massing, and materials. In general, a parking structure should not exceed the height of the principal building(s) that it serves.
- b. Laminate (i.e., wrap) parking structures with active uses along the main street retail level facade. In other areas, where structures are not laminated, compatible architectural treatment, graphic art displays and / or landscape screening (i.e., perimeter planting or green screens) is required. See Section 03.11.000 (c) for treatment options.
- c. Screen and/or locate vehicular ramps within the parking structure so that they are not readily visible or expressed on the facade of the parking structure fronting a street.
- d. Make sure that pedestrian entries to the parking structure are clearly defined, and that stairwells and elevators may be readily identified. These areas should be safe and user-friendly, designed for effective surveillance.
- e. Consider Crime Prevention Through Environmental Design (CPTED) principles in the design of all parking structures to permit active surveillance from the street.

03.15.007 Surface Lots

- a. Permanent Parking Lots

Use landscaping to break up large expanses of paved area and to shade surface lots per the Chula Vista Landscape Manual and Design Manual. Exceptions to this requirement for certain permanent parking lots and temporary parking lots are noted below.

It is expected that some permanent parking lots will ultimately be replaced by structured parking or additional buildings as the EUC intensifies over time. This type of permanent lots may exist for a number of years as a surface parking lot depending on market conditions, and will be depicted on the comprehensive block plan at the time of application for Design Review, and will be approved as part of the Design Review

application for the site. Primary considerations for these types of lots is to encourage future intensification of a site by avoid excessive redevelopment costs, while at the same time, decreasing heat island effects and ensuring that these longer-term facilities do not negatively affect surrounding properties or create a negative visual element when viewed from adjacent public streets.

Reduced landscape requirements will be applied to those permanent parking lots, or portions of lots, which are: 1) not visually prominent when viewed from public streets (Examples are lots which are located behind buildings and not directly visible from public streets or where significant grade differences exist that limit views into the lot from adjacent streets; and, 2) which are shown on a comprehensive block plan to be ultimately replaced by structured parking or future buildings. Reductions that can be considered include: total landscaped area / coverage (up to a 25% reduction); requirements for landscaped perimeter strips or islands (not adjacent to public streets); tree sizes, etc. Should future intensification not result in the replacement of these parking lots with structured parking or buildings, then full landscaping requirements shall be applied to those lots as a condition of approval of a Design Review Application for full build-out of the site.

- b. Temporary Parking Lots - Temporary parking lots are surface parking lots used on a temporary basis to provide additional parking to support existing development or to facilitate construction phasing. Because of their short-term nature, permanent landscape and improvements will not be required. Temporary parking lots are subject to the approval of a Temporary Use Permit, unless approved as part of a Design Review Application, and may be approved for an initial term of up to 3 years, with future extensions as approved by the City.

Temporary parking lots shall be paved with either asphalt or a stabilized permeable paving material consistent with Section 19.62.100 (b) of the Municipal Code, and subject to the City's stormwater ordinances. Temporary public pedestrian access shall be provided to ensure adequate connections between adjacent uses are maintained. Parking lots shall include marked parking spaces, contain directional and informational signage and have security lighting.

Screening of the lot from the public street must be provided using at least two of the following techniques:

- i. Decorative fences or screening, 36" in height, to screen cars at the perimeter.
- ii. Perimeter tree plantings. Trees shall be minimum 24" box size specimens and can remain in raised containers and be used for future planting elsewhere (phased contract growing). Temporary irrigation must be provided. Trees shall be planted at a rate of one tree for each ten spaces located along the periphery of any side of the parking lot fronting a public street.

- iii. Art or graphics screens as a means of enhancing temporary fencing, the parking lot or the paving

The method of screening shall be shown on the plans submitted with the application.

- c. Street Fronting Surface Parking Lots - Permanent surface parking lots should be located to avoid fronting on the EUC primary grid of public streets, which includes A,B,C,F and K Streets. For all other streets, permanent surface parking lots may not exceed 50% of the frontage of any block and may not be located across the street from another permanent street fronting surface parking lot. Exceptions to this are noted in Section 03.11.000.c.2 of the Form Based Code.

Temporary Parking lots are not subject to this limitation, and Temporary Parking Lots are exempt only if the temporary nature of the street fronting lot can be demonstrated through comprehensive block planning.

03.15.008 Lighting

- a. Provide lighting in all parking areas to ensure an adequate level of security.
- b. Direct and shield lighting associated with parking facilities away from adjacent residential properties.

03.15.009 Parking Management

The following principles shall apply to all projects in the EUC and shall be used to guide the development and implementation of a Parking Management Plan

- a. Principles - General
 - i. On-street metered parking should be considered throughout the project (especially the main street district) when the districts have become established and can support the imposition of metered parking.
 - ii. All parking revenues, including enforcement revenues, should go to the parking district, a business improvement district or other entity and be used for parking management or maintenance activities within the district.
 - iii. All parking facilities intended for the public shall comply with ADA standards and shall be accessible and barrier free. Parking stall and aisle dimensions shall be per the standards in Table III-B and Table III-C.
- b. Principles – Non- Residential

- i. Encourage grouping users to creatively advance the construction of shared parking structures. All conceptual block planning exercises will evaluate structure parking, as feasible, when future intensification of a site is expected to occur in the future. The use of in-lieu fees may be considered as an option by the Parking District as a mechanism for advancing structured parking.
- ii. All projects generating a need for more than 100 parking spaces, based on the initial parking rates above or those in an approved Parking Management Plan, shall prepare a parking study to identify their parking needs and to detail shared parking strategies.
- iii. Prior to the establishment of on-street metered parking, consideration should be given to posting time limits within retail parking areas to encourage turn over of prime on-street parking spaces and to discourage employee parking during hours of peak demand.
- iv. Consider the development of an on-street parking meter system for managing parking in retail areas when the retail area has become established and stabilized.
- v. Large employers (those with 100 or more employees) shall be actively encouraged to develop programs to promote off-peak hour commuting and to reduce parking demand by subsidizing transit passes, providing showers and bicycle facilities or participating in Ridelink or other Transportation Demand Management (TDM) programs sponsored by the local or regional transportation planning agencies. Examples of TDM programs currently administered as part of SANDAG's Ridelink program include: the Regional Vanpool Program, Regional Bike Locker program, Employer Outreach program, Transit pass programs, Telework / Flex Time program support, Carpool Matching and the Guaranteed Ride Home program.
- vi. On street parking spaces along the perimeter of a public park shall be allocated for public use only, and may require restrictions to ensure adequate parking is available for park users. Options may include imposition of two hour time restrictions on weekdays (and weekends if necessary), or prohibition of parking before 10:00 a.m. on weekdays to prevent office workers from parking in these spaces. Final measures shall be identified in the Parking Management Plan and may be modified or adjusted as the plan is updated.
- vii. All projects located within the Gateway Commercial Mixed Use District, the Main Street District and the Mixed Use Civic/Office Core District shall be required to participate in a comprehensive shared parking program and be included in the EUC parking district.

c. Principles – Residential

All residential projects shall institute a residential parking permit program to avoid spillover parking impacts from adjacent districts. The parking permit program shall apply to the public street fronting the project. The permit program shall include an enforcement program carried out by the HOA, a private enforcement entity, or the City.

03.15.010 Parking District Formation and Responsibilities

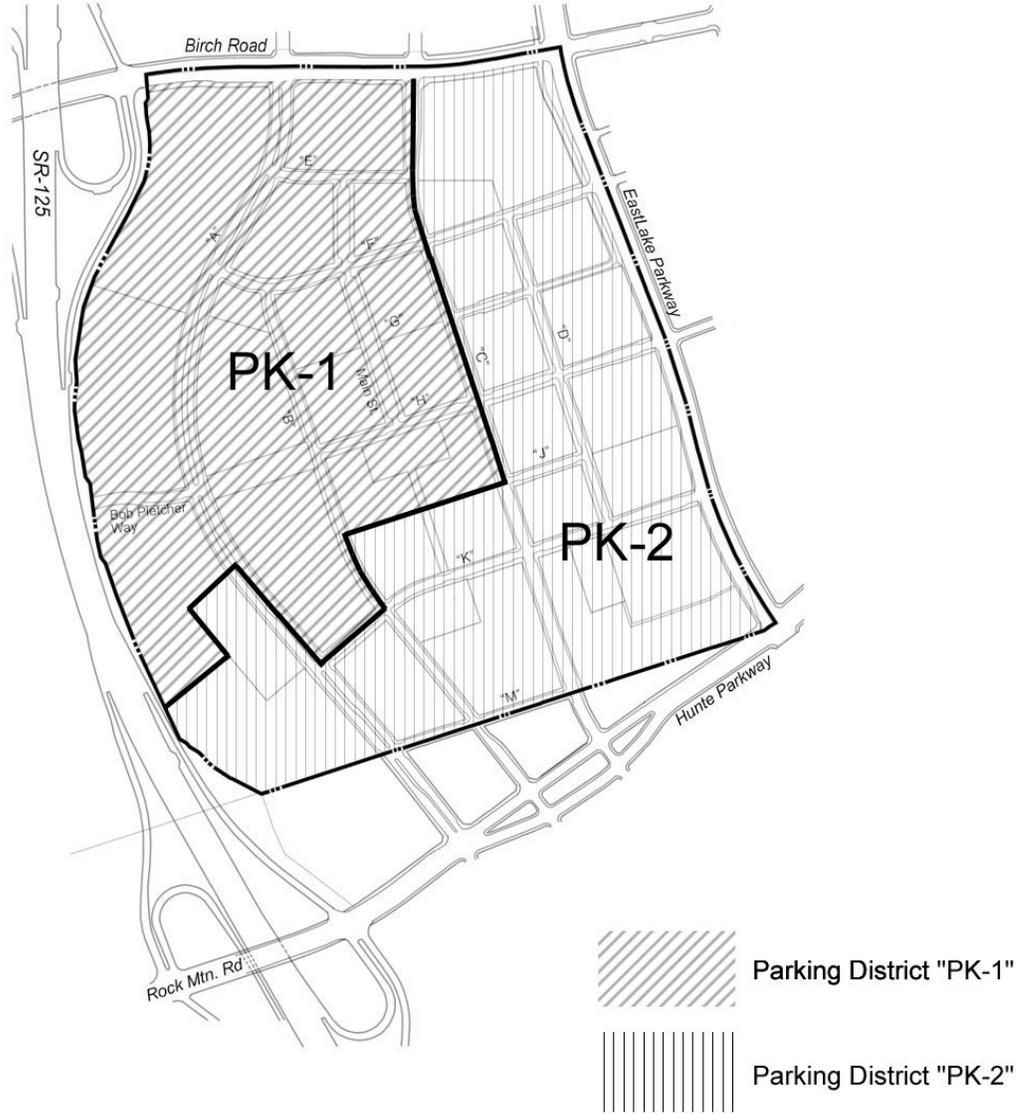
A Parking District shall be formed in the EUC and a management entity put in place to actively implement, update and enforce the parking management plan.

- a. Prior to approval of the first project in the EUC, a parking district (“District”) shall be formed by the project applicant to cover the Gateway Commercial Mixed Use District, the Business District, the Main Street District and the Mixed Use Civic/Office Core District and the residential districts, (Exhibit III-84 – Conceptual Parking Districts). The district may be formed in a phased manner or comprised of multiple zones.
- b. The City would approve the charter of the District and grant the district the authority to manage parking supply; conduct parking studies; evaluate conformity of development projects to the parking plan; set parking fees; establish and collect parking fines; implement enforcement measures; collect and spend in-lieu parking fees and allocate revenues on parking and pedestrian related improvements consistent with the goals of the parking management plan.
- c. The City would also receive the District’s updated parking plan every two years and would, at the City’s discretion, have the ability to appoint a staff member to sit on the District board.
- d. The District or similar management entity shall be responsible for preparing, implementing and enforcing a Parking Management Plan for the EUC. The initial Parking Management Plan shall be approved no later than the first occupancy in the project.
- e. The District will have the right to set fees (including in-lieu fees), establish charge and set time limits to regulate on-street parking and in shared lots and structures. The District will be empowered by the City to enforce those regulations. Any individual business desiring to set parking fees in excess of those established by the District shall be required to gain District approval.
- f. The District shall conduct an inventory and occupancy study every two years to determine the number of available spaces in the District during peak periods and to evaluate shared parking effectiveness. The study shall be used to update the Parking Management Plan, including the development of new parking ratios for projects within the District. The new ratios would apply to all development projects in the district during the following two year period.
- g. All projects generating a need for more than 100 parking spaces, based on the initial

parking rates above or those in an approved Parking Management Plan, shall prepare a parking study to identify their parking needs and to detail shared parking strategies. All projects will be required to obtain approval of their specific parking plan from the District and will be issued a letter of concurrence for submittal to the City at the Design Review stage of their approval process. Projects generating a need for 100 spaces or less shall use the initial parking rates listed in Table III-A or those in an approved Parking Management Plan, to determine adequate parking supply.

- h. All residential projects shall institute a parking permit program to avoid spillover parking impacts from adjacent districts. The parking permit program shall include the public streets fronting the project and may include internal private streets at the developer's discretion. The program shall also include an enforcement program to be carried out by the District, the HOA, a private enforcement entity or the City.
- i. Prior to occupancy, all projects located within the area of the district shall be formally annexed into the District.

Parking Districts



Note: Boundaries of the Parking Districts are conceptual



Eastern Urban Center OTAY RANCH



Exhibit III-84